



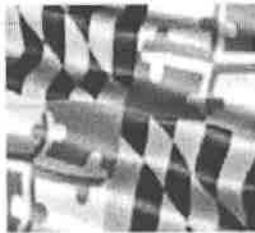
**BALTIMORE COUNTY FIRE DEPARTMENT
FIRE-RESCUE ACADEMY**

1545 Sparrows Point Boulevard, Sparrows Point, Maryland 21219
Phone: (410) 887-7523 Fax: (410) 887-7572

**MARYLAND
MOTOR CARRIER
HANDBOOK**

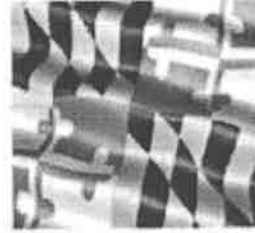
**FIRE DEPARTMENT
OPERATIONS**

Desire. Determination. Dedication.



MDOT
 MARYLAND DEPARTMENT
 OF TRANSPORTATION

STATE HIGHWAY
 ADMINISTRATION



Maryland Motor Carrier Handbook
 Revised January, 2018

In Cooperation with:

- ◆ Maryland Port Administration*
- ◆ Maryland Transportation Authority*
- ◆ Maryland State Police*
- ◆ Motor Vehicle Administration*
- ◆ Public Service Commission*
- ◆ Comptroller of Maryland*
- ◆ Maryland Department of the Environment*
- ◆ Maryland Department of Transportation*

Maryland Virtual Weigh Station Technology



Vehicle ID: 20-0000070-09-21-118120
 Date: 03/09/2019 09:30 AM
 Location: I-66 N 25th Milepost 99.9 Road 20
 Lane: 1
 Weight: 17244.651
 Length: 60.17
 Width: 13
 Step: 1.5
 *This is an approximate weight and length based on the truck's load. Actual weight and length may vary.

Axis	4.4	4.4	PKT	4.5	17
WT	10.2	15.3		30.4	15.6

Disclaimer:

Information contained in the Handbook regarding the various laws and regulations governing commercial motor vehicle operations in Maryland are subject to change without notice. The Handbook is produced solely as a convenience to the public and the State assumes no warranty or representation, either expressed or implied, regarding the information given or the use of any of the material provided or for unintentional omissions, errors, or misprints which appear in the Handbook.

On The Cover: Maryland's Virtual Weigh Station Program is designed to monitor select roadways to assure that vehicles comply with size and weight laws. Enforcement personnel are able to use wireless technology to access the sites remotely and can identify and stop violators.





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**COMMERCIAL
DRIVER'S LICENSE
OVERVIEW**

information to obtain a USDOT number and biannual USDOT Number update information to them.

Carriers and entities that have been issued a USDOT Number must provide the FMCSA with updated information every two years. Those who fail to provide the updated information will have their USDOT Number deactivated. The registrant is not allowed to begin operations until an active USDOT Number is approved by FMCSA. For additional information visit the FMCSA website at:

<http://www.fmcsa.dot.gov>

CHAPTER VIII

DRIVER'S LICENSING REQUIREMENTS

A. GENERAL RULES

Every person driving a motor vehicle, except those expressly exempted, must have in their possession a valid driver's license of the appropriate class for the type of vehicle or vehicle combination driven. Under specific conditions, if you wish to drive a commercial motor vehicle

William Donald Schaefer Tower
Transportation Division
6 St. Paul Street
Baltimore Maryland 21202
Telephone # 1-410-767-8128

D. UNIFIED CARRIER REGISTRATION ACT (UCRA)

The "*Unified Carrier Registration Act*," (UCRA), which became effective in the latter part of 2007, is completely different than the Unified Registration System (URS). The UCRA replaced the Single-State-Registration Program. Under the UCRA, all motor carriers, brokers, freight forwarders, and persons leasing vehicles over 10,000 pounds were required to pay a registration fee for power units (**trucks, truck tractors, and buses**) over 10,000 pounds that operate interstate commerce.

Maryland does not participate in the UCRA, but Maryland motor carriers, brokers, freight forwarders and individuals leasing trucks, truck tractors and buses over 10,000 pounds which are used in interstate commerce, (that travel out of state) will be required to register under the UCRA. For additional information, visit the following website:

<http://www.ucr.in.gov>

E. UNIFIED REGISTRATION SYSTEM (URS)

In November 2013, the Federal Motor Carrier Safety Administration (FMCSA) amended their registration system to require interstate motor carriers both private and for-hire carriers as well as freight forwarders, brokers, intermodal equipment providers (IEPs), hazardous materials safety permit applicants, and cargo tank facilities to submit required registration

(CMV), you will be required to obtain a Commercial Driver's License (CDL) (certain conditions apply) and have it in your possession.

A person who is licensed in another state and establishes residency in Maryland is required to obtain a Maryland driver license within 60 days. **Within 30 days of establishing a residence in this State, the holder of a CDL shall apply for a Maryland CDL.**

For additional information, call the MVA telephone numbers listed in Appendix C or visit their website at:

<http://www.mva.maryland.gov>

B. COMMERCIAL DRIVER'S LICENSE (CDL)

For the purposes of the CDL License requirement in Maryland, a "commercial motor vehicle" (CMV) means:

1. A motor vehicle with a gross vehicle weight rating (GVWR) of 26,001 pounds or more;
2. A combination of vehicles with a gross combination weight rating (GCWR) of 26,001 pounds or more if the GVWR of the vehicle being towed is in excess of 10,000 pounds;
3. Any vehicle which is designed to transport 16 or more passengers, including the driver;
4. A vehicle of any size and is used in the transportation of materials found to be hazardous for the purposes of the "*Hazardous Materials (Haz-Mat) Transportation Act*," and which requires the motor vehicle to be placarded under hazardous materials regulations ("*49 CFR Part 172, Subpart F*"). (Reference source: "*The Maryland TA, Title 16, §803(c)(iv)*.")

Remember: For the purposes of determining if a CDL is required regarding weight, the following applies:

Gross Vehicle Weight Rating (GVWR) means:

- **The value specified by the vehicle manufacturer as the loaded weight of a single vehicle**

Gross combination weight rating (GCWR) is the greater of:

- (1) **A value specified by the manufacturer of the power unit,**
Or
- (2) **The sum of the gross vehicle weight ratings (GVWRs) or the gross vehicle weights (GVWs) of the power unit and the towed unit(s), or any combination thereof that produces the highest value. Exception: The GCWR of the power unit will not be used to define a commercial motor vehicle when the power unit is not towing another vehicle.**

The definition for a CMV does not include the following for the purposes of a CDL. However, a driver who is operating any of the vehicles listed below must have a Non-Commercial Driver's License with the appropriate license class:

a. A vehicle controlled and operated by a farmer:

- Used to transport agricultural products, farm machinery, or farm supplies to or from a farm.
- Not used in the operations of a common or contract motor carrier, and
- Used within 150 miles of the person's farm.

b. An emergency vehicle:

- Equipped with audible and visual signals, and
- Operated by a member of, or a person in the employ of a volunteer or paid fire or rescue organization.

c. A vehicle owned or operated by the United States Department of Defense if it is controlled and operated by:

- Any active duty military personnel, or

- Any member of the military reserves, or National Guard on active duty, including personnel on full-time National Guard duty and personnel on part-time training, or
 - Any National Guard Military Technician (NGMT).
- d. A motor vehicle designed and constructed primarily to provide temporary living quarters for recreational, camping, or travel use.**

C. AGF REQUIREMENTS FOR A CDL

<p>At age 18 with the proper Class of CDL*:</p> <ol style="list-style-type: none"> 1. I am allowed to drive only in Maryland. 2. I cannot drive interstate. 3. I cannot drive a vehicle hauling Hazardous Materials (Haz-Mat). 4. I cannot drive a vehicle carrying 16 or more passengers including the driver. 5. I cannot drive a School Bus. <p>*You must have a current medical certificate on file with MVA</p>	<p>At age 21 with the proper Class of CDL*:</p> <ol style="list-style-type: none"> 1. I can drive both intrastate and interstate. 2. I can drive the following vehicles <u>ONLY</u> when I have the appropriate endorsement on my CDL: <ul style="list-style-type: none"> • Tank Vehicle • Vehicle carrying Haz-Mat • School Bus • Vehicle carrying passengers (16 or more including the driver) • Double combinations <p>*You must have a current medical certificate on file with MVA</p>
<p>Note: Interstate transport is the pick-up or delivery of cargo originating or destined <u>outside</u> the State of Maryland (pier freight, air freight and some warehouse freight can be interstate cargo). Intrastate transport is the pick-up or delivery of cargo originating and destined <u>within</u> the State of Maryland.</p>	

Commercial Driver's License (CDL) Requirements

CDL Applicant's current license status	MD Law Test	CDL Knowledge Test(s)	CDL Instructional Permit	CDL Skills Test	Remarks
No License	Yes	Yes	Yes	Yes	
Non-commercial Maryland Driver's License or lower class CD	No	Yes	Yes	Yes	
Existing MD License – Converting to CDL of same class	No	Yes	Yes	Yes	CDL skills tests may be waived by MVA
Out-of-state Driver's License (Non CDL)	No	Yes	Yes	Yes	Drivers will be required to obtain an instructional permit
Out-of-state CDL of same class	No	See remarks	No	No	Haz-Mat test required when converting out of state CDL to MD, and at renewal, and whenever Haz-Mat endorsement is being added to CDL
Out-of-state CDL of lower class	No	Yes	Yes	Yes	

All applicants must pass a vision-screening test. To prepare for the written Maryland Law Test, you should obtain and read the "Maryland CDL Handbook."

A vehicle and trailer used for testing purposes (CDL) must have the manufacturer's plate on the vehicle and trailer. This plate is normally located inside the door frame on the driver's side of the vehicle and on the front of the trailer. The manufacturer's plate (label) provides the gross vehicle weight rating (GVWR) for the vehicle and in some instances provides the gross combination weight rating (GCWR) or towing capacity for the vehicle as well.

D. THE CDL DRIVING TEST

CDL examinations are conducted at various MVA locations. **The skills test is conducted by appointment only!** You **must** apply in person at a full service MVA branch office to go through the process for obtaining a CDL. The applicant **must** present a current medical examiner's certificate provided by a certified medical examiner (DOT physical) prior

to taking any CDL knowledge test(s) and **must** take and successfully pass all written tests. You may obtain information concerning locations, dates and business hours at:

<http://www.mva.maryland.gov>

The applicant is required to supply a registered motor vehicle in safe operating condition of a type to test the applicant's ability to drive all vehicles under that class of CDL. (Refer to the chart on the next page)

Class "A", and "B" or "C" with passenger endorsement will require a CDL skills test, which consists of a pre-trip inspection, on course maneuvers and public road test. You must successfully pass the pre-trip inspection before moving onto the course maneuvers. Further, you must successfully pass the course maneuvers test before moving onto the public road test. All of the tests are designed to assess your driving ability, knowledge and observance of traffic laws.

A driving skills test will not be conducted in a vehicle with defective equipment. While driving with a learner's instructional permit, you must be accompanied by a person properly licensed to drive vehicles of that class or higher class who has at least three years driving experience and is at least 21 years of age. When appearing for your driving skills test, you must be accompanied by a person who is properly licensed to drive vehicles of that class, or you may be driven to the location where the testing will be performed by a person who meets the conditions as previously listed. ***NOTE: CDL skills tests are not given at all locations**

If you have any questions call the MVA at the telephone numbers listed in Appendix C, or contact them at:

<http://www.mva.maryland.gov>

Driving Test for CDL – Vehicle Type Required

CDL - Class "A" Truck tractor/trailer (Class "F" truck tractor and Class "G" semi-trailer) equipped with air brakes is the preferred motor vehicle. Other vehicle combinations with a GCWR of 26,001 pounds or more may be used if the towed vehicle has a GVWR in excess of 10,000 pounds. *

If a combination other than an air brake equipped vehicle is used, your license will be restricted to vehicles without air brakes. **

CDL – Class "B" A vehicle with a GVWR of 26,001 or more pounds. **

CDL – Class "C" A vehicle with a GVWR of less than 26,001 pounds.

NOTE: You may be issued a Class "M" (motorcycle) license with any other license. However, you will be issued only one license – for example, on the face of the CDL it would show A M, indicating you can drive any vehicle in either Class A, B, C (with proper endorsement if required), or a motorcycle.

*For this combination you will be issued a Class "A" CDL; however, you will have an "O" restriction on it indicating you are not licensed to drive a truck tractor-semi-trailer combination.

** If you take the test using a vehicle without air-brakes, you will have an air-brake restriction placed on your CDL indicating you are not licensed to drive air-brake equipped vehicles.

Maryland Commercial Driver's Licenses

With this Class	You may drive	And may tow	You may not drive the following listed vehicles
A	Any single vehicle or combination of vehicles	Any trailer*	• Motorcycles

B	Motor vehicles 26,001 or more pounds (GVWR)*	Trailers not in excess of 10,000 pounds GVWR*	<ul style="list-style-type: none"> • Combinations of Class "F" truck tractor and Class "G" trailer • Combinations of vehicles where the GCWR is 26,001 pounds or more and towing a vehicle in excess of 10,000 pounds GVWR • Motorcycles
C	Motor vehicles less than 26,001 pounds (GVWR)*	Trailers not in excess of 10,000 GVWR	<ul style="list-style-type: none"> • Combinations of Class "F" truck tractor and Class "G" trailer • Vehicles with a GVWR 26,001 pounds or more • Any combination of vehicles with a GCWR of 26,001 pounds or more and towing a vehicle in excess of 10,000 pounds • Motorcycles



***Notes:** Endorsements are required for Tank Vehicles, Doubles, Hazardous Materials, Passengers, and School Buses. Refer to the next page.

E. SPECIAL ENDORSEMENTS FOR CDL'S

A special endorsement is required on the CDL to drive any of the following listed vehicles.

Special Endorsements

- **N** – Tank Vehicle- A liquid or liquid gas in a tank or tanks having an individual rated capacity of more than 119 gallons and an aggregate rated capacity of 1000 gallons or more that is either permanently or temporarily attached to the vehicle or the chassis.
- **P** - Passenger vehicles – 16 or more passengers including the driver
- **S** - School Bus
- **T** - Double trailer combination
- **H** -Vehicles transporting Hazardous Materials (Haz-Mat) which requires placarding
- **X** - N and H combined

Commercial Driver's License (CDL)		
CDL ENDORSEMENTS		
Double/Triple Trailers (T) 	Passenger Vehicles (P) 	Tank Vehicles (N) 
Hazardous Materials (H) (any size vehicle) 	School Buses (S) 	Tank & Hazardous Materials (X) 
Federal Motor Carrier Safety Administration		U.S. Department of Transportation www.fmcsa.dot.gov

F. HAZARDOUS MATERIALS (Haz-Mat) ENDORSEMENTS

Any CDL driver seeking to add a Haz-Mat endorsement to their CDL, or renewing a CDL that has an existing Haz-Mat endorsement, or possessing a CDL with a Haz-Mat endorsement issued by another state and is seeking to obtain a Maryland CDL with a Haz-Mat endorsement is subject to a background check and other processes as required by the "U.S. Patriot Act of 2001." For additional information, visit the MVA website at:

<http://www.mva.maryland.gov>

G. DRIVER'S RESPONSIBILITY

All commercial drivers must do the following:

1. Notify their employer of any convictions of motor vehicle laws, other than parking violations, within 30 days of the date of conviction; and
2. Notify their employer of any revocation, suspension, cancellation or disqualification, before the end of the business day following the day that the driver received notice of the fact; and
3. Provide any new employer with a 10-year employment history and a three year driving history; and
4. Surrender to the MVA all driver's licenses issued by any other state or jurisdiction.

All CMV drivers who fit the criteria listed in item B1, B2, B3, and B4 of this section must have in their possession when driving a CMV a valid CDL for the class of vehicle being driven, and current medical examiner's certification (DOT physical).

H. NON-COMMERCIAL DRIVER'S LICENSE

Non-commercial Class "A" and "B" Driver's License examinations are conducted at various MVA locations by appointment only! You may obtain information regarding locations, dates and business hours from any branch office or at:

<http://www.mva.maryland.gov>

The applicant is required to supply a registered motor vehicle in safe operating condition of a type to test the applicant's ability to drive all vehicles under that class of Non-commercial Driver's License. Non-commercial Class "A" and "B" Driver's License tests will include a pre-trip equipment inspection by the applicant in the presence of an MVA Driver's License Examiner.

A driving skills test will not be conducted in a vehicle with defective equipment. While driving with a learner's instructional permit, you must be accompanied by a person properly licensed to drive vehicles of that class or higher class who has at least three years driving experience and is at least 21 years of age.

When appearing for your driving skills tests, you must be accompanied by a person who is properly licensed to drive vehicles of that class, or you may be driven to the location where the testing will be performed by a person who meets the conditions as previously listed.

Drivers of Non-CDL type commercial vehicles that have a GVWR or GVW of 10,001 pounds up to 26,000 pounds are required to be physically qualified to drive them. In this case, a driver must have in their possession a current medical examiner's certification (DOT physical) card or a copy of the card

**Driving Test for a Non-commercial Driver's License -
Vehicle Type Required**

Non-commercial Class "A"	Non-commercial truck tractor (Class "F" vehicle) and trailer (Class "G" vehicle).
Non-commercial Class "B"	Non-commercial motor vehicle with a gross vehicle weight rating (GVWR) 26,001 pounds or more, or a combination of vehicles (except a Class "F" truck tractor and Class "G" trailer), which has a gross combination weight rating (GCWR) of 26,001 pounds or more.

Non-commercial Class "C"	Automobile, station wagon, light truck, or any non-commercial motor vehicle with a GVWR of less than 26,001 pounds.
Non-commercial Class "M"	Motorcycle.

Non-commercial Driver's License

With this Class	You may drive	And may tow	You may not drive the following listed vehicles.
A	Any Non-commercial vehicle	Any Non-commercial trailer	<ul style="list-style-type: none"> • Commercial motor vehicles • Motorcycles
B	Any single Non-commercial vehicle with a GVWR of 26,001 pounds or more, or combination of vehicles with a GCWR of 26,001 pounds or more	Any Non-commercial trailer	<ul style="list-style-type: none"> • Commercial motor vehicles • Class "F" truck tractor in combination with a Class "G" trailer • Motorcycles
C	Any single Non-commercial vehicle with a GVWR of less than 26,001 pounds or combination of vehicles with a GCWR of less than 26,001 pounds	Any Non-commercial trailer as long as the vehicle combination GCWR is less than 26,001 pounds	<ul style="list-style-type: none"> • Commercial motor vehicles • Class "F" truck tractor & Class "G" trailer • Vehicles and vehicle combinations where the GVWR is 26,001 pounds or more • Motorcycles
M	Motorcycles	M/C trailer	Any other vehicle

I. QUESTIONS

If you have any questions regarding either CDL or Non-Commercial Driver's License requirements for vehicles, or where the MVA Branch Offices are located, call the MVA telephone numbers listed in Appendix C.

Or visit their website at:

<http://www.mva.maryland.gov>

J. SUSPENSIONS/REVOCATIONS/DISQUALIFICATION

1. Suspensions

Suspension of a Maryland driver's license and/or privilege means to temporarily withdraw the privilege to operate a motor vehicle. Periods of suspension vary under specific circumstances. Should you have questions as to your driving status, call the MVA telephone numbers in Appendix C or visit an MVA office in your area.

2. Revocations

Revocation of a Maryland's driver's license and/or privilege means to temporarily withdraw the privilege to operate a motor vehicle. Similar to the suspension process the revocation of a driver's license and/or driving privilege is considered more serious in nature. Periods of revocation vary.

3. Disqualification

Disqualification of a Maryland CDL and/or privilege means to withdraw the privilege to operate a CMV. Periods of disqualification can vary; however, for some offenses/violations the period of disqualification can be for life.

When a Maryland driver's license and/or driving privilege is refused, canceled, suspended or revoked, the license and/or driving privilege is terminated. The person is not permitted to drive a motor vehicle under ANY circumstances. Similarly, if a person has been disqualified in Maryland from driving a CMV, the person may not drive a CMV under ANY circumstances.

***NOTE-** Some serious violations committed in your personal car may disqualify you from driving a CMV.

If your license is refused, canceled, suspended, or revoked in any state or jurisdiction, you are not allowed to drive any vehicle in Maryland under any circumstances. If you have been disqualified from driving a CMV in any state or jurisdiction, you are not allowed to drive a CMV in Maryland under any circumstances.

For reinstatement procedures, visit an MVA branch office in your area, or call the MVA telephone numbers listed in Appendix C.

CHAPTER IX

INSURANCE REQUIREMENTS

A. INTRASTATE OPERATIONS

1. FOR CARRIERS OF HAZARDOUS MATERIALS:

Carriers hauling Hazardous Materials that are required to be marked and placarded shall comply with levels of liability as contained in the *Federal Motor Carrier Safety Regulations (FMCSR)*, Part 387. See paragraph B on the next page.

2. FOR CARRIERS OF PASSENGERS:

Liability Insurance:

- A. The minimum per accident insurance required for each vehicle with a seating capacity of 7 PASSENGERS OR LESS is \$50,000 for injury to one person, \$100,000 for injuries to two or more persons, and \$20,000 for property damage OR \$120,000 combined single limit.
- B. The minimum per accident insurance required for each vehicle with a seating capacity of BETWEEN 8 AND 15 PASSENGERS is \$75,000 for injury to any one person, \$200,000 for injuries to two or more persons, \$50,000 for property damage OR \$250,000 combined single limit, and
- D. Minimum per accident insurance required for each vehicle with a seating capacity of 16 PASSENGERS OR MORE is \$75,000 for injury to any one person, \$400,000 for injuries to two or more persons, and \$100,000 for property damage OR \$500,000 combined single limit.

3. FOR-HIRE CARRIERS OF PROPERTY

For-hire vehicles engaged in intrastate commerce that exceed a gross vehicle weight rating of 26,000 pounds and are designed to carry property are required to maintain the minimum levels of insurance as contained in the *FMCSR, Part 387*. See paragraph B below.

B. INTERSTATE OPERATIONS

**SAFE
VEHICLE
OPERATIONS**

outreach by providing carriers safety information and training to achieve voluntarily compliance and improved safe operation.

CHAPTER XI

SAFE VEHICLE OPERATIONS

A. VEHICLE CONTROL

Exercising safe control over a vehicle as it is being operated on highways in Maryland is the responsibility of the driver. Safe operation of a vehicle not only saves time, but it saves lives.

1. Driving loaded vehicle down long hills or steep grades:

- Control the speed of your vehicle by shifting to a lower gear at the top of the hill
- Brake smoothly while proceeding downward

Provides motor carrier safety inspectors with accurate and timely information where data is used to determine if a safety inspection needs to be conducted at the TWIS or at roadside. In the long run, this saves time for inspectors and motor carriers because it focuses on non-compliant carriers and enables compliant carriers to by-pass the TWIS.

- **Roadside Electronic Screening (ES)**

This process allows compliant carriers to by-pass a TWIS and focuses more on non-compliant carriers.

A. CURRENT CVISN APPLICATIONS

To participate in the Drivewyze ES System, go to their website at:

<http://www.drivewyze.com>

Or for additional information call them at: 1-888-988-1590.

B. CVISN – THE FUTURE

CVISN is an evolving program. New and innovative enhanced technologies are constantly being examined and tested. These include Virtual Weigh Stations, license plate and USDOT Number reader technology and wireless inspections for future applications.

You can find more information about CVISN on the FMCSA website:

<http://www.fmcsa.dot.gov>

C. VIRTUAL WEIGH STATIONS

The Office of Traffic & Safety, Motor Carrier Division (MCD), has developed a network of electronic commercial motor vehicle (CMV) safety monitoring sites known as Virtual Weigh Stations (VWS). While one of the primary functions of these sites is to monitor CMV weight activity, they are capable of providing an abundance of other safety data such as speed, size and unsafe driving practices. Twenty-one sites are currently operational on high volume truck routes to create an electronic safety web across the State. Analytical data generated by the VWS assists law enforcement in establishing CMV specific safety strategies as well as highway planners and engineers towards preserving the integrity of our roadway system. Data from the VWS network also provides MCD a continuous opportunity for educational

- Do not pump the brake pedal, but apply constant, even pressure

2. Approaching off-ramps:

When approaching exit-ramps, slow the speed of your vehicle and use caution upon entering and traveling through the ramp. Not all exit-ramps are the same. Some have sharper turns than others, so please use caution.

B. LOAD SECUREMENT

When transporting logs, poles, lumber, pipe, or steel on an open bed truck or trailer you are required to at a minimum:

- Securely fasten them to the vehicle at the front and rear of the load with approved tie-downs to include two chains or other approved fastening devices with equal tensile strength around the front and rear of the load. The chains must be at least ½ inch in diameter, common coil B.B. chains for loads over 3 tons, and 3/8-inch diameter for 3 tons or less.

Other fastening devices for use include 5/16-inch diameter wire rope, synthetic fiber webbing and steel strapping.

Additional load securement requirements apply and information concerning these can be found in the "Federal Motor Carrier Safety Regulations (FMCSR), Part 393.100." For additional information or guidance, you should consult the FMCSR, Part 393.100 through 136. If you still have questions, contact either the SHA Motor Carrier Division at 1-410-582-5734 or toll-free at 1-800-543-4564, or the MSP/CVED at 1-410-579-5959.

If a load extends over the rear of a vehicle more than 4 feet or 4 inches beyond either side of the vehicle, a red or orange fluorescent flag at least 18 inches square and a red light are required to mark the extremities of the load. (Oversize Hauling Permit restrictions may also apply – refer to Chapter XIII)

Spillage or residue left on the vehicle due to loading or unloading must be removed from any non-load-carrying parts of the vehicle prior to it being driven on a highway in Maryland. The tailgate must be securely

fastened and the bed may not have holes or openings through which material can escape.

C. BRAKING SYSTEMS – Maintenance & Safety

Every vehicle shall have a service braking system that will enable the driver to bring the vehicle to a safe stop and hold it no matter what its load on any grade on which it is operated that is free of ice and snow. A vehicle's braking system is one of the most critical components on a vehicle. When properly maintained, the braking system will assure adequate braking force is available for use by the driver to slow the speed of a vehicle and bring it safely to a stop. On the other hand, a poorly maintained system is a disaster waiting to happen. **Your life and the lives of other motorists are at risk if you fail to check and maintain your vehicle's brake system, and assure that it is in proper working order.**

Under normal operating conditions on a vehicle equipped with an air-brake system, the driver should not rapidly pump the brake pedal up and down, as this will reduce air pressure below the minimum pressure needed for safe operation.

If the air pressure in the brake system drops below a safe operating level, a low-pressure warning device will activate alerting the driver of a brake problem. When this happens, the driver should stop the vehicle as safely as possible and not drive it until the braking system has been repaired.

The braking system for a multiple vehicle combination shall be equipped with an emergency valve to activate the trailer/semi-trailer brakes immediately in case the trailer or semi-trailer detaches or becomes detached from the towing unit while the combination is traveling on a highway.

D. DISABLED VEHICLES

****DANGER****

Vehicles carrying explosives, or a cargo tank truck carrying flammable liquids, compressed gases, or a motor vehicle using compressed gas as a fuel are not allowed to carry:

- A flare, or
- Fusee, or
- Signal produced by flame

If your vehicle becomes disabled, move it off the road to a safe area as soon as possible. Once off the roadway place the necessary warning flares, reflectors or other signal devices on the side facing approaching/oncoming Traffic



Where do I place the emergency breakdown safety devices?

<p>On undivided or one lane roadways, three flares or warning devices must be placed:</p> <ul style="list-style-type: none"> • One beside the vehicle • One 100 feet behind the vehicle • One 100 feet ahead of the vehicle 	<p>On divided highways or one-way highways, place the three warning devices behind your vehicle facing the traffic side at the following locations:</p> <ul style="list-style-type: none"> • One 10 feet behind • One 100 feet behind • One 200 feet behind
<p>DO:</p> <ul style="list-style-type: none"> • Place break-down safety devices as required to warn approaching motorists as soon as possible • If you have a telephone, call for assistance and arrange to move as soon as possible 	<p>DON'T:</p> <ul style="list-style-type: none"> • Leave your vehicle on the road without placing safety devices • Leave your vehicle on the side of the road without making arrangements to have it removed as soon as possible

NOTE: Inoperable vehicles, motor vehicles, trailers, semi-trailers, and buses will be considered abandoned and will be towed at the owner's expense when left unattended on:

- 1) Public highways for more than 48 hours*, or
- 2) A controlled access highway for more than 24 hours*, and
- 3) Toll roads under the jurisdiction of the Maryland Transportation Authority for more than 12 hours. *

***Abandoned vehicles that pose a traffic hazard may be towed immediately.**

"A PERSON MAY NOT STOP, STAND, OR PARK A VEHICLE ON ANY ENTRANCE OR EXIT RAMP OF ANY HIGHWAY WITH TWO OR MORE LANES FOR TRAFFIC MOVING IN THE SAME DIRECTION." ("The Maryland TA, Title 21, §1003 (k)")

CHAPTER XII

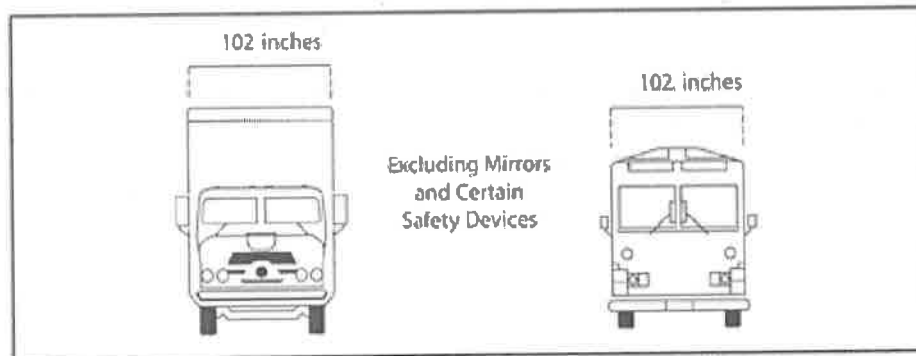
VEHICLE SIZE & LOAD

A. MAXIMUM WIDTH – MEASURING - LIMITS EXPLAINED - EXCEPTIONS

The width of a vehicle includes any load that it carries, but is exclusive of safety and energy conservation devices, such as side mirrors, turn signal lamps, marker lamps, steps and handholds for entry or egress, flexible fender extensions, mud-flaps and spray suppressant devices and load induced tire bulges. Except for side mirrors, no other safety or energy conservation device shall extend more than 3 inches on each side of the vehicle.

The width of a motor home or travel trailer shall be exclusive of retractable awnings installed by the vehicle manufacturer or dealer, provided that the awnings do not extend more than 6 inches from each side of the vehicle.

The maximum vehicle or vehicle combination width limit in Maryland is 102 inches (8 feet 6 inches). Certain roadways in the State may be posted by traffic signs prohibiting travel by 102 inch vehicles – i.e., I-895 Baltimore Harbor Tunnel where the maximum allowable vehicle/combination width is 96 inches (8 feet)



Vehicles and/or combinations that exceed 102 inches in width are required to obtain an Oversize Hauling Permit. For additional information, call the Maryland State Highway Administration's Hauling Permit Unit telephone numbers in Appendix C, or email the Unit at:

haulingpermits@sha.state.md.us

or

MdMotorCarrier@sha.state.md.us

B. MAXIMUM HEIGHT

- Maximum allowable height: 13'6"

Height detectors are installed at several TWIS as well as facilities owned by the MDTA – e.g. Baltimore Harbor Tunnel, Fort McHenry Tunnel, and elsewhere.

C. LENGTH – How to measure - Exceptions

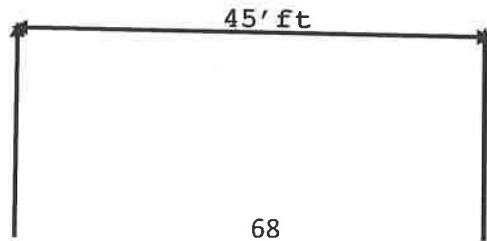
The length of a vehicle includes its front and rear bumpers and any part of its load that extends beyond the vehicle. Not included, when measuring length, are non-load-bearing safety and energy conservation devices, such as marker lamps. The overhang of an automobile transporter transporting vehicles or boats of 3 feet to front, 4 feet to rear is also not included. The load on the rear of an automobile or boat transporter including any retractable device on the rear of a combination of vehicles engaged in the transportation of automobiles in use to support a transported vehicle, may not extend more than 4 feet beyond the rear of the bed or body of the automobile transporter. The overhang of a stinger-steer automobile transporter may not extend more than 4 feet beyond the front and 6 feet to the rear.

Refer to the next page for the maximum allowable vehicle length limits .

1. MAXIMUM LENGTH LIMITS FOR SINGLE UNIT TYPE VEHICLES –

A QUICK GUIDE

Single unit truck or bus:	40'
Travel trailer:	40'
Publicly owned rigid bus:	41'
Class "M" motor home:	45'





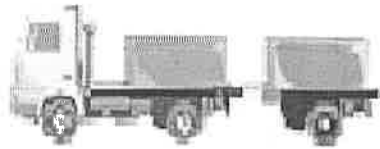
Class "M" motor home

Bus operated on highway part of the Interstate System or State

Primary System:	45'
Publicly owned articulated three-axle bus:	60'
Other vehicles:	35'

2. MAXIMUM LENGTH LIMITS FOR COMBINATIONS OF VEHICLES

Any other combination of vehicles other than those listed:	55'
Cargo carrying power unit in combination with another vehicle:	62'
Combination of non-commercial power unit and travel trailer:	65'



Single Unit Truck Pulling a Trailer (Example)

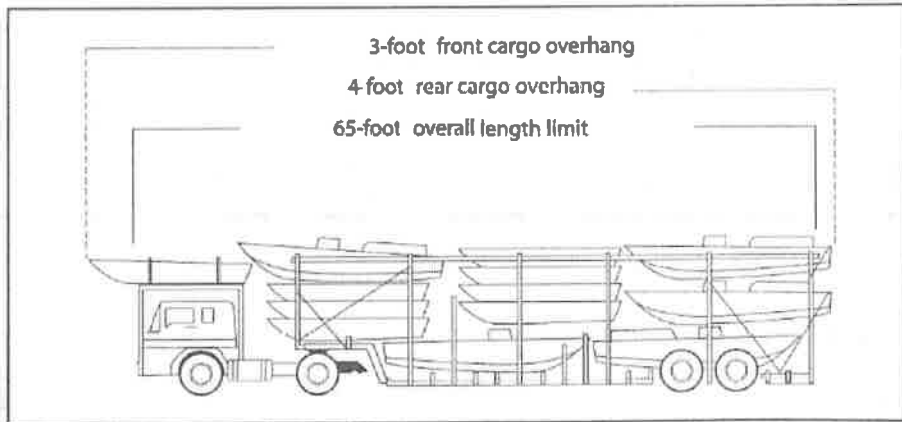
Maxi-cube vehicle consisting of
Non-articulating straight truck
with a semi-trailer that does not
exceed 34':

65'

Truck, or truck tractor-semi-trailer combination designed for and engaged exclusively
in the transportation of automobiles or boats:

65'

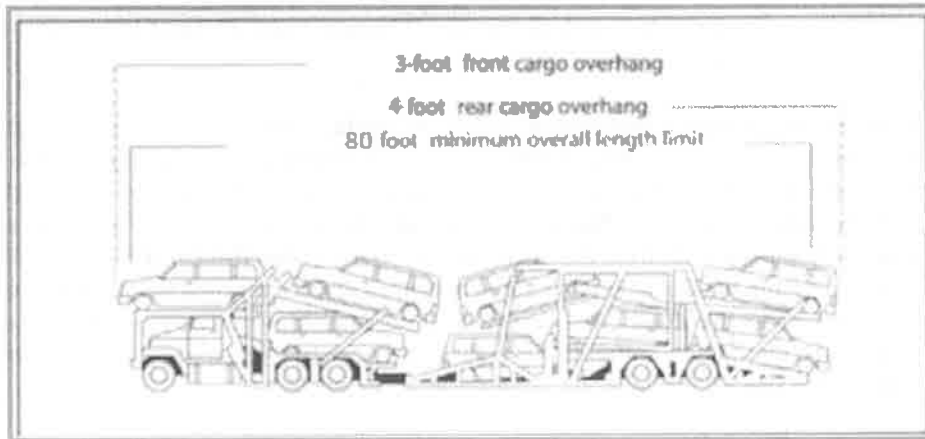
70



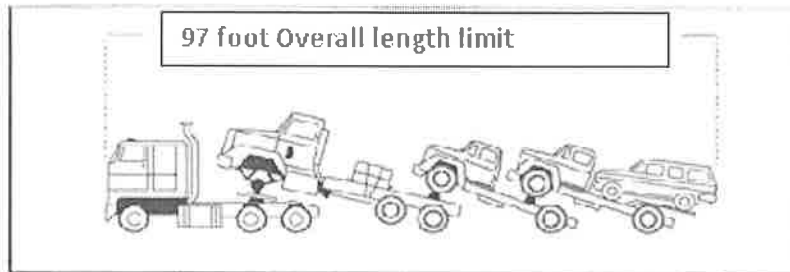
Automobile/Boat Transporter (Example)

Stinger-steer combination consisting of a truck tractor-semi-trailer carrying automobiles, or boats:

80'



Note: Overhang Correction – 4-feet in front 6 feet in rear (FAST Act)



Exceptions to Length Limitations

A combination of a semi-trailer and a trailer (double) being operated in combination with a non-cargo carrying truck tractor is not restricted to an overall length; however, the semi-trailer and trailer operated in this combination may not exceed 28 feet in length for each unit. This vehicle combination is restricted to operating on the Maryland National Network System of Highways. (Refer to Chapter II)

Vehicles or combination of vehicles carrying crew or racing shells are exempt from the length limitations, as are a combination of vehicles carrying an indivisible load, if the load is not over 70 feet long and is traveling during daylight hours.

Vehicle combinations carrying poles, piling, or mill logs are exempt from the length limitations if the poles, piling or mill logs do not exceed 75 feet in length. Before transporting poles, piling and mill logs that are over 75 feet on a vehicle combination, the owner/driver of the combination will be required to obtain an oversize hauling permit from the SHA Motor Carrier Division (MCD). The driver must have the permit in possession or be able to display when operating the combination on highways in the State. The permit is to be available for inspection by police officers. To obtain a permit visit our website at www.roads.maryland.gov/CVO or , refer to the SHA MCD telephone numbers in Appendix C.

3. SEMI-TRAILERS EXCEEDING 48 FEET IN LENGTH UP TO A LENGTH NOT TO EXCEED 53 FEET – OPERATING RESTRICTIONS

A person may operate a truck tractor in combination with a semi-trailer (single) that exceeds 48 feet in length up to a length not to exceed 53 feet, but travel for this combination is limited to specific routes. Additional restrictions apply.



The following conditions apply:

- **Travel is restricted to the National Network System of Highways and the shortest practical route between a designated highway and a truck terminal, port, or point of origin or destination. Refer to Chapter II for approved routes and additional restrictions.**
- The wheelbase of the semi-trailer, measured as the distance from the kingpin to the center of the rear tandem axles, may not exceed 41 feet in length.
- The kingpin setback, measured as the distance from the kingpin to the front of the semi-trailer may not exceed 4 feet in length.
- The rear overhang, measured as the distance from the center of the rear tandem axles to the rear of the semi-trailer, may not exceed 35 percent of the wheelbase of the semi-trailer.
- The width of the semi-trailer shall be at least 96 inches and not more than 102 inches.
- The distance between the outside edges of the semi-trailer's tires shall be equal to the width of the semi-trailer.

a. The semi-trailer shall be equipped with:

- (1) Vehicle lights which comply with or exceed federal standards; and
- (2) Reflective material that is consistent with the standards for conspicuity promulgated by the National Highway Traffic Safety Administration if manufactured after December 31, 1993; and
- (3) The semi-trailer shall be equipped with a rear under-ride guard of sufficient strength to prevent a motor vehicle from penetrating underneath the semi-trailer.

Federal Regulations require each trailer and semi-trailer with a gross vehicle weight rating of 10,001 pounds or more and manufactured after Jan 26, 1998, to meet more stringent standards. Rear under-ride guards may not be more than:

- 22" from the ground
- 4" from the side of the trailer
- 12" from the rear of the trailer

4. MAXIMUM BUMPER HEIGHTS

Class "E" trucks, with a manufacturer's rating or registered gross weight of more than 10,000 pounds, but not more than 18,000 pounds, may not be operated on any highway with a bumper that exceeds a clearance of 30 inches from the ground. If the truck is used for spraying agricultural crops, the maximum allowable height is 32 inches.

D. LOAD EXTENSION LIMITS - EXCEPTIONS

The load on any vehicle or a combination of vehicles may not extend more than 3 feet beyond the front of the vehicle or 6 feet to the rear of the vehicle. Except for an automobile or boat transporters.

Vehicles carrying wooden prefabricated roof trusses, in an inverted position, are permitted a maximum rear extension of 10 feet. Combinations of vehicles carrying indivisible loads during daylight hours are allowed provided the load is not over 70 feet in length.

Vehicle combinations transporting poles, piling and mill logs over 75 feet in length are required to obtain an oversize hauling permit prior to movement. This new provision took effect April 8, 2013. To obtain a

permit, call the SHA MCD Hauling Permits Section. Refer to Appendix C for the contact information.

Also exempt from the overhang restrictions are vehicles or combination of vehicles carrying piling, poles, mill logs, nursery stock or crew or racing shells.

The overall maximum length restrictions vary for particular vehicles. For additional questions regarding this matter, call the SHA Motor Carrier Division at the telephone numbers listed in Appendix C.

E. COVERING LOADS

The beds on all vehicles transporting loose materials that include dirt, sand, gravel, wood chips, or other material that can blow, fall or spill from a vehicle as a result of movement or of exposure to air, wind, or weather, shall have its bed fully enclosed on all sides if any part of the load is higher than the enclosure or touches the enclosure within 6 inches of the top, the load must be covered with a canvas or other type of approved cover.

Vehicles exempt from covered load law

- **Class "K" Farm area.**
- **Class "E" truck with manufacturer's rated capacity of 3/4 ton or less, and a maximum gross vehicle weight of 7,000 pounds or less.**
- **Construction vehicle working within confines of a public works construction project site as outlined in the construction project's plans and specifications, provided the distance traveled does not exceed 1 mile or the distance specified in an extension granted.**
- **Construction vehicle or mining equipment while crossing a highway between construction and mining sites.**
- **All Class "G" trailers provided no part of the load is higher than six inches below the top of any of the enclosures.**
- **Within the Port of Baltimore for a distance not to exceed 1 mile, any vehicle carrying a load of loose material between a stockpile or storage facility and a vessel docked at the port**

Dropping sand to improve traction or spreading water or other substance to clean or maintain a highway is allowed. **Agricultural products in their natural state or residue from processed vegetable products used as feed for animals that are being transported to a farm**

are also exempted. The vehicle owner is responsible for removing spilled agricultural products from the highway.

For additional information, please call either the Maryland State Highway Administration's Motor Carrier Division or the Maryland State Police Commercial Vehicle Enforcement Division telephone numbers listed in Appendix C.

CHAPTER XIII

OVERSIZE/OVERWEIGHT HAULING PERMITS

A. LEGAL WEIGHTS

**VEHICLE
SIZE AND
LOAD
OPERATIONS**

Baltimore, Maryland 21224
Telephone # **1-410-537-1374** or **1-888-754-0185 (toll-free)**

MDTA website address:

<http://www.mdt.maryland.gov>

E. TOLL FACILITIES – TUNNELS – BRIDGE RESTRICTIONS

The permittee or user shall notify the Maryland Transportation Authority Police (MDTAP) (specific locations listed below), or the Maryland State Police (MSP)(I-95) and speak to the Duty Officer at least one hour before beginning movement of an OS/OW vehicle on or through the facility. The MDTAP or the MSP at the JFK Highway (I-95) may be required to provide police escort for certain OS/OW Hauling Permit loads. The permittee is responsible for any additional fees that may be charged for required police escort.

**(1) I-895 - Baltimore Harbor Tunnel (MDTAP)
Telephone # 1-410-537-1230**

- (2) I-95 - Fort McHenry Tunnel **(MDTAP)**
Telephone # 1-410-537-1230

- (3) US 301 - Harry W. Nice Memorial Bridge
(Potomac River Bridge) **(MDTAP)**
Telephone # 1-410-537-6800

- (4) US 50/301 - William Preston Lane, Jr.,
Memorial Bridge (Bay Bridge) **(MDTAP)**
Telephone # 1-410-537-6601
or
1-410-757-1977

- (5) I-695 - Francis Scott Key Bridge **(MDTAP)**
Telephone # 1-410-537-7600

- (6) US 40 - Thomas J. Hatem Memorial Bridge **(MDTAP)**
(Susquehanna River Bridge)
Telephone # 1-410-537-1196

- (7) I-95 - John F. Kennedy Memorial Highway **(MSP)**
Telephone # 1-410-537-1150

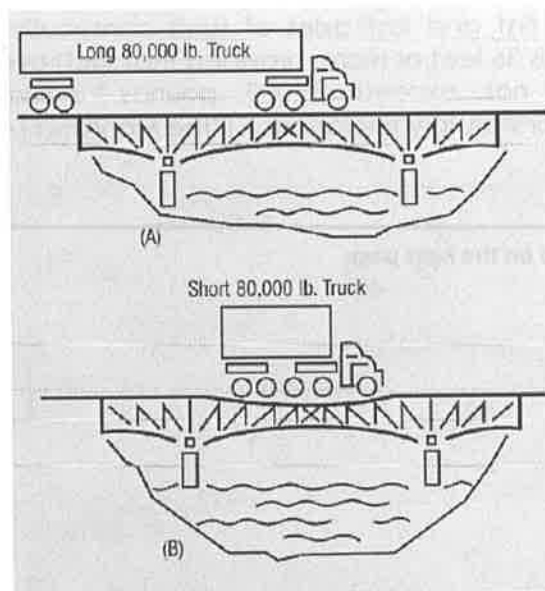
The MDTA has comprehensive restrictions on the movement of Hazardous Materials (Haz-Mat) as defined in the *"Hazardous Materials Regulations, Title 49, Code of Federal Regulations (CFR), Part 171.8,"* across highways under their jurisdiction

CHAPTER XIV

MAXIMUM ALLOWABLE VEHICLE & AXLE WEIGHTS

Maryland has adopted the Federal Bridge Formula (FBF) and Table regarding the maximum allowable vehicle axle/gross weights. The FBF was developed by the Federal Highway Administration (FHWA), to minimize damage to bridges and pavements caused by vehicles or combinations having too short a distance between the axles when carrying/hauling a heavy load across a bridge or structure. **Maryland does not allow loading tolerance errors on the Interstate Highways.**

A. BRIDGE FORMULA



The above figures (A & B) represent what the Bridge Formula is designed to accomplish. A shorter wheelbase means you are not allowed to carry a heavier weight. This is because more stress is produced on a structure – such as a bridge when the weight of a vehicle is concentrated on a short wheelbase. A shorter wheel-based vehicle carrying a heavy load would cause substantial damage to the bridge.

The formula is as follows:

$$W = 500 \left[\left(\frac{LN}{N-1} \right) + 12N + 36 \right]$$

"Where..."

"**W**" = Overall gross weight on any group of 2 or more consecutive axles to the nearest 500 pounds,

"**L**" = Distance in feet measured horizontally between the vertical center lines of the extreme of any group of 2 or more consecutive axles, and

"**N**" = Number of axles in a group under consideration, except that 2 consecutive sets of tandem axles may carry a gross load of 34,000 pounds each providing the overall distance

between the first and last axles of such consecutive sets of tandem axles is 36 feet or more; provided that such overall gross weight may not exceed 80,000 pounds, including any enforcement or statutory tolerances." (*The Maryland TA, Title 24, §109 (b)*)

Refer to the Bridge Table on the next page

BRIDGE TABLE
DISTANCE IN FEET BETWEEN THE EXTREMES OF ANY
GROUP OF 2 OR MORE CONSECUTIVE AXLES

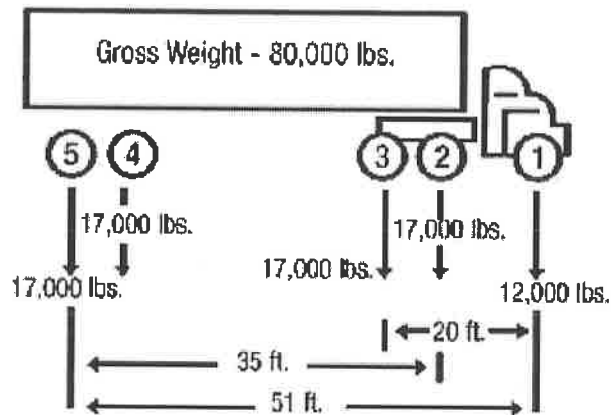
	2 axles	3 axles	4 axles	5 axles	6 axles	7 axles
4	34,000					
5	34,000					
6	34,000					
7	34,000					
8	34,000	34,000				
and less						
More than	58,000	42,000				
8						
9	39,000	42,500				
10	40,000	43,500				
11		44,000				
12		45,000	50,000			
13		45,500	50,500			
14		46,500	51,500			
15		47,000	52,000			
16		48,000	52,500	58,000		
17		48,500	53,500	58,500		
18		49,500	54,000	59,000		
19		50,000	54,500	60,000		
20		51,000	55,500	60,500	66,000	
21		51,500	56,000	61,000	66,500	
22		52,500	56,500	61,500	67,000	
23		53,000	57,500	62,500	68,000	
24		54,000	58,000	63,000	68,500	74,000
25		54,500	58,500	63,500	69,000	74,500
26		55,500	59,500	64,000	69,500	75,000
27		56,000	60,000	65,000	70,000	75,500
28		57,000	60,500	65,500	71,000	76,500
29		57,500	61,500	66,000	71,500	77,000
30		58,500	62,000	66,500	72,000	77,500
31		59,000	62,500	67,500	72,500	78,000
32		60,000	63,500	68,000	73,000	78,500
33			64,000	68,500	74,000	79,000
34			64,500	69,000	74,500	80,000
35			65,500	70,000	75,000	
36			(66,000)	70,500	75,500	
37			(66,500)	71,000	76,000	
38			(67,500)	72,000	77,000	
39			68,000	72,500	77,500	
40			68,500	73,000	78,000	
41			69,500	73,500	78,500	
42			70,000	74,000	79,000	
43			70,500	75,000	80,000	
44			71,500	75,500		
45			72,000	76,000		
46			72,500	76,500		
47			73,500	77,500		
48			74,000	78,000		
49			74,500	78,500		
50			75,500	79,000		
51			76,000	80,000		
52			76,500			
53			77,500			
54			78,000			
55			78,500			
56			79,500			
57			80,000			

*Note exception to the Bridge Formula in Section C

B. EXAMPLES – HOW TO DETERMINE GROSS VEHICLE WEIGHT

AND MULTIPLE AXLE WEIGHTS USING THE BRIDGE FORMULA

The vehicle with weights and axle dimensions as shown in Figure 1, illustrates a Bridge Formula check.



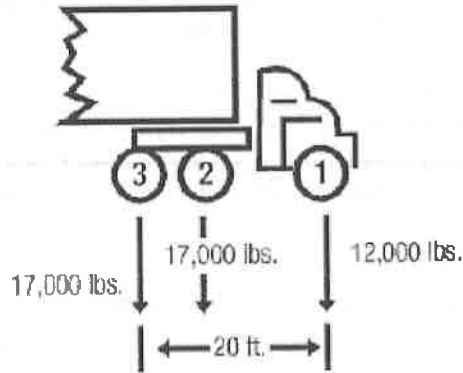
(Figure 1)

Before checking axle 1 through 3 spacing, check to see that single, tandem and gross weights are satisfied. The single axle Number 1 should not exceed 20,000 pounds, tandem axles should not exceed 34,000 pounds, and the gross weight should not exceed 80,000 pounds (refer to the Bridge Table to assure the vehicle has enough axles and spacing that allows the maximum gross vehicle weight (GVW) of 80,000 pounds).

In Figure 1, the 12,000-pound weight for the steering axle is given as an example. Maryland allows a maximum gross axle weight limit of 20,000 pounds on a single axle under the following circumstances:

1. The vehicle is registered for more than 73,000 pounds; and
2. The sum of the rated load capacities for each tire on the axle do not exceed the stated capacity (weight); or
3. The sum of the rated load capacities indicated by the manufacturer as to each tire on the axle with which the vehicle was originally equipped is not exceeded.

These requirements are satisfied, so the next thing you need to check is shown in Figure 2:



(Figure 2)

Check # of axles 1 through 3

Actual weight = 12,000 + 17,000 + 17,000 = 46,000 pounds.

N = 3 axles

L = 20 feet

$$W = 500 \left[\frac{LN}{N-1} + 12N + 36 \right]$$

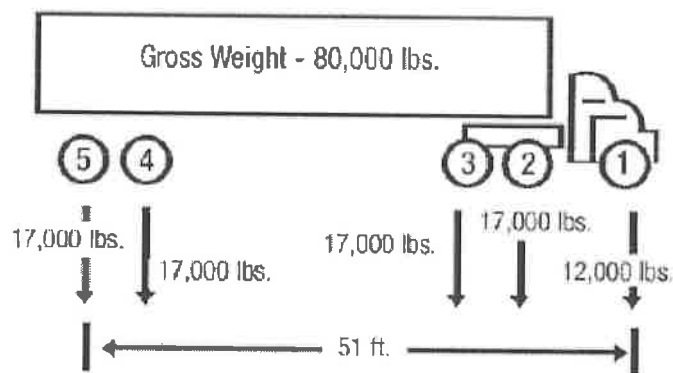
$$W = 500 \left[\frac{(20 \times 3)}{(3 - 1)} + (12 \times 3) + 36 \right] = 51,000 \text{ lbs.}$$

W maximum = 51,500 pounds which is more than the actual weight of 46,000 pounds so the Bridge Formula requirement is satisfied.

In the previous example, if axles 2 and 3 are 17,000 pounds each (34,000 pounds total), the steering axle could be 17,500 pounds and the truck would be legal if the axle distances remain the same and the **tire load rating on the sidewall indicates this weight may be carried.** If the tire

rating (both front tires) indicates the rating for each is 7,000 each, then the front axle would only be allowed a maximum gross weight of 14,000 pounds regardless of the axle spacing (Bridge Formula).

Taking the same number (51,500 pounds' gross weight) from the previous example obtained from Bridge Table as shown by reading down the left side to $L=21$ and across to the right where $N=3$, you can determine the overall gross weight limits (including axle weights), by following the below listed example



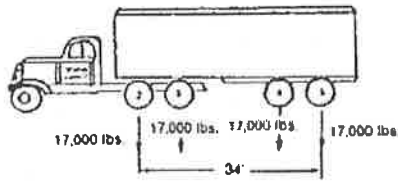
(Figure 3)

Now check axles 1 through 5 to determine the maximum allowable gross vehicle weight (GVW). (Refer to Figure 3 above)

$$W \text{ (actual)} = 12,000 + 17,000 + 17,000 + 17,000 + 17,000 = 80,000 \text{ pounds.}$$

W maximum, from the Bridge Table for L of 51 feet and

N of 5 = 80,000 pounds. (Includes 1,000-pound tolerance*) The measurement of 51' which is made from the center of the hub on the steering axle of the truck, to the center of the hub on the last axle of the trailer is commonly referred to as the "exterior (or outer) bridge." Therefore, this axle spacing is satisfactory.



(Figure 4)

Next check axles 2 through 5 (Shown in Figure 4 above).

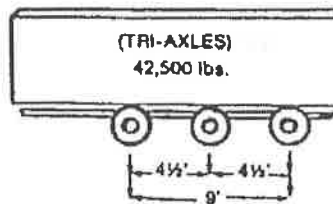
$$W \text{ (actual)} = 17,000 + 17,000 + 17,000 + 17,000 = 68,000 \text{ pounds.}$$

W maximum, from the Bridge Table for "**L**" of 34 feet and "**N**" of 4 = 64,500 pounds. The distance measurement (shown in Figure 4), is taken from the center of axle #2 to the center of axle #5. This measurement is commonly referred to as the "interior (or inner) bridge."

The measurement of 34' reveals an interior bridge weight violation because the actual weight exceeds the maximum allowed weight for the given axle spacing. To correct the situation, some of the load must be removed from the truck or the axle spacing (34-foot dimension) increased.

The measurement of 34' reveals an interior bridge weight violation because the actual weight exceeds the maximum allowed weight for the given axle spacing. To correct the situation, some of the load must be removed from the truck or the axle spacing (34-foot dimension) increased.

Bridge Formula Example for Three Consecutive Axles



(Figure 5)

If the distance in Figure 5, measured from the center of the first axle to the center of the last axle of the semi-trailer is 9' then a maximum allowable tridem axle (3 axles) weight of 42,500 pounds would be allowed.

C. EXCEPTION TO THE BRIDGE FORMULA

There is an exception to the Bridge Formula. That is, two or more consecutive sets of tandem axles spaced more than 40 inches apart, but not more than 96 inches apart may carry a gross load of 34,000 pounds each, providing the overall distance between the first and last axles of such consecutive sets of tandem axles is 36 feet or more. This is the exception stated in the Bridge Table which refers to paragraph (b) of the "Maryland TA," Title 24, §109.

For example, a 5-axle truck-tractor-semi-trailer is allowed a maximum tandem axle weight of 34,000 pounds on the tandem of the tractor (axles 2 and 3) and 34,000 pounds on the tandem of the trailer (axles 4 and 5), provided there is an overall spacing of 36 feet or more between axles 2 and 5 for a weight of 68,000 pounds for these two sets of tandems.

A spacing of 36 feet or more for axles 2 through 5, is satisfactory for an actual weight of 68,000 pounds for the two sets of tandem axles even though the Bridge Table shows an allowed weight of 66,000 to 67,000 pounds for spacing of 36 feet to 38 feet. Again, note the exception shown in the Bridge Table.

Remember, the exception granted under law is for two or more consecutive sets of tandem axles spaced more than 40 inches apart, but not more than 96 inches apart providing the overall distance between the first and last axles of such consecutive sets of tandem axles is 36 feet or more. For example, if the distance between axles 4 and 5 is more than 96" (8'), then the tandem would not qualify as part of the 2 consecutive sets of tandems and would be considered one tandem by itself. Under this circumstance, the Bridge Formula weight of 66,500 pounds would apply if the distance between axles 2 through 5 was 37 feet, and it would not fall within the exemption.

Should you have any questions regarding the measurements for axles, weight laws, etc., please call the SHA Motor Carrier Division telephone numbers listed in Appendix C.

D. GROSS WEIGHT LIMITATIONS BASED ON THE NUMBER OF AXLES

The Gross Vehicle Weight (GVW) of any vehicle or combination of vehicles as shown below may not be exceeded

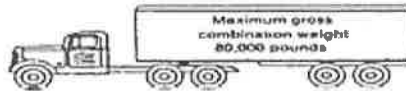
Number OF AXLES	GROSS VEHICLE WEIGHT OR GROSS COMBINATION WEIGHT (IN POUNDS)
THREE	55,000
FOUR	66,000
FIVE OR MORE	80,000



3-axle vehicle



4-axle combination



5-axle combination

The allowable weights for the vehicles or vehicle combinations listed above will be determined based on the vehicle meeting the Bridge Formula.

E. WEIGHT ENFORCEMENT PROGRAM

The Maryland State Police, Commercial Vehicle Enforcement Division (MSP/CVED), and the Maryland Transportation Authority Police, Commercial Vehicle Safety Unit (MDTAP/CVSU), conduct truck size and

weight enforcement operations at Truck Weigh and Inspection Stations (TWIS) throughout Maryland. The MDTAP/CVSU normally conducts operations on loll facilities only. Both departments also conduct size and weight enforcement operations through the use of roving patrols equipped with portable scales.

Seven local enforcement likewise conduct roving operations for weight enforcement utilizing portable scales. In all, there are over 500 certified portable scales in use by all Maryland enforcement agencies.

The fine for refusing to submit to weighing and measurement is \$1,000.00, plus any court costs. A loading error or tolerance of 1,000 pounds is allowed, but not given on Interstate highways.

Except as otherwise provided in "*The Maryland TA, Title 24, § 111.1,*" a vehicle found to exceed any allowable weight limit by 5,000 pounds or more may not be moved until the excess weight is unloaded.

CHAPTER XV

MOVEMENT OF HAZARDOUS MATERIALS (HAZ-MAT)

A. GENERAL INFORMATION

**TRAFFIC
CRASH
REPORTING**

Carriers: Must be registered through the E-Modal System.

For more information visit their website at:

<http://www.emodal.com>

For additional information call: 1-866-758-3838

Or email them at: customersupport@emodal.com

Drivers: To enter the Port must have a current Transportation Worker Identification Credential (TWIC). For more information visit the following website at:

<http://www.tsa.gov/twic>

or

<http://homeport.uscg.mil/twic>

If you do not have a TWIC you will need to be escorted into the Port terminal. For addition information, visit:

www.marylandports.com

At the site use the drop-down menu and look for Port Security.

CHAPTER XVIII

TRAFFIC CRASH REPORTING

GENERAL REQUIREMENTS

Maryland law requires the driver of a vehicle involved in a crash to do several things. **If you are involved in a crash you are required to stop immediately, return to the scene and remain there until the following requirements have been fulfilled:**

- 1) Give reasonable assistance to any person injured.
- 2) Give your name, address, and the registration number of the vehicle you are driving.

- 3) On request show your driver's license, if it is available, to any person injured in the crash, the driver, occupant, or person attending any vehicle or property damaged in the crash.
- 4) Provide the owner or driver with your name and address, and if you are driving someone else's vehicle, the name and address of owner of the vehicle as well as the registration number for the vehicle.
- 5) In the case of unattended vehicles or property attempt to locate the owner.
- 6) If the driver, owner, or person in charge of the damaged vehicle or other property cannot be located, you are required to leave in a conspicuous, secure place in or on the damaged vehicle or other property a written notice containing the same information.
- 7) Give the name and address of your insurance carrier or other security provider, along with a policy or other identifying number of the liability insurance, if available, and name and address of the local agent or local office of your insurance carrier, if available.
- 8) If your vehicle strikes and injures a domestic animal, the driver of the motor vehicle shall immediately notify the appropriate State or local police of the crash.

Motor carriers are no longer required to submit accident (crash) reports to the Federal Motor Carrier Safety Administration (FMCSA), and notify the agency of fatal accidents. Instead, the FMCSA currently depends on reports from the enforcement agencies that investigate the accident.

The motor carrier is required to maintain for a period of one year after an accident occurs, an accident register containing at least the following information:

- a. A list of accidents containing the date, city or town and state in which the crash occurred, driver name, number of injuries, number of fatalities and whether hazardous materials (other than fuel spilled from the fuel tanks of the vehicles involved in the crash) were released.

- b. Copies of all accident reports required by State or other governmental entities or Insurers.

"FMCSR," defines an accident (crash) as:

- **A fatality, or**
- **Bodily injury to a person who immediately receives medical treatment away from the scene of the accident**
- **One or more motor vehicles incurring disabling damage as a result of the accident requiring the vehicle be towed or transported from the scene by a tow truck or other vehicle**

CHAPTER XIX

MARYLAND MOTOR CARRIER SAFETY PROGRAM (MMCSP)

A. PURPOSE OF PROGRAM

The Maryland Motor Carrier Safety Program (MMCSP) in the State has evolved since its inception in 1985. The number of safety inspections conducted on buses, trucks, drivers and their cargos per year has increased from 4,978 in 1986, to 118,870 in 2017.

Goals of the MMCSP

- **Reduce traffic crashes involving CMV's**
- **Reduce deaths, injuries and property loss resulting from them**
- **Reduce the risk involving the transportation of Haz-Mat**

**FEDERAL
MOTOR
CARRIER
SAFETY
REGULATIONS**

CHAPTER XX

FEDERAL MOTOR CARRIER SAFETY REGULATIONS (FMCSR)

A. GENERAL ISSUES

Maryland enforces the following "FMCSR, Title 49, Code of Federal Regulations," (CFR) which was adopted into the "Code of Maryland Regulations (COMAR), Title 11":

- *Part 40*
- *Parts 107 through 180* - Transportation of Hazardous Materials (Haz-Mat)
- *Part 382* - Drug and alcohol testing
- *Part 383* - Commercial Driver's Licenses
- *Part 385, Subparts A, C, and D* - New Entrant Safety Assurance Program
- *Part 386, Subparts F and G* - Injunctions and Imminent Hazards, Penalties
- *Part 387* - Levels of insurance. See Chapter IX for additional information
- *Parts 390 through 399*

- c. Truck tractors.
- d. Freight trailer or semi-trailer, and
- e. Buses designed and used to carry more than 10 people and not regulated by "*The Maryland TA, Title 25, §110.*"

The vehicle's owner must provide certification when registering a new vehicle that they are in compliance with the Program. Violation of the PM Program can subject the owner to fines up to \$520.00, as well as possible suspension of the vehicle registration by MVA. **Further, a copy of the vehicle inspection is required to be kept in the vehicle.**

Where can I get a PM Handbook?

**Office of the Secretary of State
Division of State Documents
16 Francis Street
Jeffrey Building
Annapolis, Maryland 21401-1924**

The Division of State Documents charges a nominal fee for the Handbook. For information call them at 1-410-974-2486 or toll-free (within Maryland only) at 1-800-633-9657, or visit their website at:

<http://www.dsd.state.md.us>

The FMCSA has certified that the Maryland PM Program meets the requirements of the federal annual inspection process as contained in the "*FMCSR, Part 396.17.*"

"The Maryland TA, Title 25, §111," governs Maryland's enforcement of the FMCSR, regarding commercial motor vehicles (CMV). Noncompliance can result in a maximum fine of \$1,020.00.

The regulations apply to the following vehicles:

- Trucks
- Truck tractors
- Buses
- Trailers, semi-trailers, pole trailers
- Converter dollies
- Combinations of vehicles if the gross vehicle weight ratings (GVWR) exceed 10,000 pounds
- All interstate and intrastate motor carriers of property or passengers

For further information, call the SHA Motor Carrier Division or the Federal Motor Carrier Safety Administration (FMCSA) telephone numbers listed in Appendix C.

For vehicles transporting Haz-Mat that require the vehicle to be marked or placarded in accordance with the "CFR, Parts 177-180," or for vehicles designed to transport 16 or more occupants, drivers must comply with all of the FMCSR, including medical examination and certification requirements contained in Part 391, regardless of vehicle size/weight.

Copies of the FMCSR are available for sale by the Maryland Motor Truck Association (Refer to Appendix C) and the US Government Printing Office.

The following summarization of the FMCSR adopted by Maryland applies to motor carriers, officers, agents, representatives, supervisors, and employees responsible for the management, maintenance, operation, or driving of motor vehicles.

B. PART 391 - QUALIFICATIONS OF DRIVERS

Interstate Drivers

To drive a CMV interstate you must:

- Be at least 21 years old, and
- Be able to read and speak the English language sufficiently to converse with the general public, understand highway traffic signs and signals, respond to official inquiries, and:
- Have a current medical card, and
- Possess a valid license with a proper class from only one state
- When required have the proper endorsement on the CDL

The medical examination and certification requirements contained in the "FMCSR, Part 391.41 (a), 391.43 and 391.45," apply to both interstate and intrastate drivers who operate a CMV, or CMV combination with a gross vehicle weight rating (GVWR), or gross vehicle weight (GVW), or combination weight rating (GCWR), or gross combination weight (GCW) of 10,001 pounds, or more.

Interstate drivers who are not medically qualified to operate a CMV and who wish to drive are required to obtain a waiver from the FMCSA. For questions regarding medical waivers, call the FMCSA at 1-443-703-2240.

A drug and alcohol testing program for interstate and intrastate drivers with a CDL operating a CMV requiring a Commercial Driver's License (CDL) is in force in Maryland as explained in Chapter XXI.

Intrastate Drivers

Intrastate drivers of CMV's with a GVWR, or GCWR of 10,001 pounds or more are required to have a current medical examiner's certificate, or copy in their possession certifying they are qualified to drive them.

Intrastate drivers who are not medically qualified to operate a CMV and who wish to drive are may apply to obtain a waiver from the Maryland Motor Vehicle Administration. For questions regarding medical waivers, call the MVA at 410-768-7662.

An intrastate driver who does not meet the physical qualifications under the FMCSR, Part 391.41 (b)(1-11), may still drive a CMV if:

1. Qualified to drive a CMV, and was driving a CMV before October 1, 2003; and
2. Not transporting Haz-Mat; and

3. Not operating a vehicle carrying 16 or more passengers including the driver; and
4. A disqualifying physical condition does not get worse.

Drivers must carry a current medical card or copy thereof and be retested every two years. For more information, call the SHA Motor Carrier Division telephone numbers in Appendix C.

C. PART 392 - DRIVING OF COMMERCIAL MOTOR VEHICLES (CMV'S)

<p>Carriers shall not:</p> <ul style="list-style-type: none"> • Allow the operation of a CMV while the driver's ability or alertness is impaired through: <ol style="list-style-type: none"> 1. Fatigue, or 2. Illness, or 3. Any other cause. 	<p>Drivers shall not:</p> <ul style="list-style-type: none"> • Operate a CMV while their ability or alertness is impaired through: <ol style="list-style-type: none"> 1. Fatigue, or 2. Illness, or 3. Any other cause. • While on-duty or driving, possess, consume, be under the influence of or use: <ol style="list-style-type: none"> 1. An intoxicating beverage, or 2. Prohibited drugs. • Consume an intoxicating beverage within 4 hours before going on duty or operating, or having physical control of a CMV.
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Before driving, drivers shall perform a pre-trip inspection on the vehicle to make sure it is in safe operating condition. At a minimum, the driver must check the following parts and accessories:

1. Brakes - service, trailer connections, parking (hand).
2. Steering mechanism.
3. Lighting devices and reflectors.
4. Tires.
5. Horn.
6. Windshield wiper or wipers.
7. Rear vision mirror or mirrors, and
8. Coupling devices.

The following emergency equipment shall be carried on all vehicles in the proper place, and shall be ready for use by the driver:

- Fire extinguisher having a rating of 5 B: C or more – properly filled, readily accessible, securely mounted, and
- Spare fuses or overload protection device,
- Three emergency, bi-directional, reflective triangles (fuses, liquid burning flares, or red electric lanterns may be used instead of triangles). **However, flame producing devices are prohibited for certain types of Haz-Mat transport.**

D. PART 393 - PARTS AND ACCESSORIES NECESSARY FOR SAFE OPERATION

Part 393, details the requirements for lighting devices, reflectors, electrical equipment, brakes, glazing/window construction, fuel systems, coupling devices and towing methods, emergency equipment, protection against shifting or falling cargo, load securement utilizing certain equipment, and other miscellaneous parts and accessories (e.g., tires, mirrors, horn, rear end protection, projecting loads, seat belt assemblies, etc.). No vehicle shall be operated unless it is equipped with these devices. All parts and accessories shall at all times be in safe and proper operating condition.

E. PART 395 - HOURS OF SERVICE (HOS) FOR DRIVERS

The following HOS apply to the drivers of property carrying CMV's traveling interstate. *

11-hour driving limit	14 hour on-duty limit	60/70 hour on-duty limit	Sleeper berth provision
<p>May drive a maximum of 11 hours' after 10 consecutive hours off-duty</p> <p>*See Rest Break Provision below!</p>	<p>May not drive beyond the 14th hour after coming on-duty following 10 consecutive hours off-duty</p>	<p>May not drive after 60/70 hours on-duty in 7/8 consecutive days. Can restart a 7/8-day period after taking 34 consecutive hours off-duty.</p>	<p>Drivers using the sleeper berth provision must take at least 8 consecutive hours in the sleeper berth, plus a separate 2 consecutive hours either in the sleeper, off-duty, or any combination of the two</p>

Rest Breaks – You may drive only if 8 hours or less have passed since the end of your last off-duty or sleeper berth period of at least 30 minutes. *

The rest break provision will not be enforced for drivers staying within 100 air-miles or non-CDL drivers who stay within 150 air-miles.

*Information taken from FMCSA's information card – "Summary of Hours-Of-Service (HOS) Final Regulations." [FMCSA-ADO-13-006C]

For additional information on the regulations for the HOS visit the FMCSA website at:

www.fmcsa.dot.gov

HOS for Drivers of Passenger Carrying Vehicles – Interstate

The following HOS apply to the drivers of passenger carrying vehicles traveling interstate. *



10-Hour Driving Limit	15-Hour Driving Limit	60/70-Hour On-Duty Limit	Sleeper berth provision
May drive a maximum of 10 hours after 8 consecutive hours off-duty	May be on-duty for not more than 15 hours. After reaching the 15th hour, the driver may not drive again until they have had at least 8 consecutive hours off-duty	May not drive after 60/70 hours on-duty in 7/8 consecutive days	Drivers using the sleeper berth must take at least 8 hours in the sleeper berth, and may split the sleeper berth time into two periods provided neither is less than 2 hours**

*Information taken from FMCSA's information card – "Summary of Hours-Of-Service (HOS) Final Regulations." [FMCSA-ADO-13006C]

**** Most buses are not equipped with qualifying sleeper berths.**

When required, the Record of Duty Status (Log Book) must:

1. Be maintained for each 24-hour period, in duplicate, current to the last change of duty status; and
2. Be carried by the driver and contain the log for the previous 7 days; and
3. Be retained by the carrier for six months.

Refer to the FMCSR, Part 395.3 and Part 395. 8.

Short Haul Provisions - Driver of Property-Carrying Vehicles - Interstate

There are two short haul provisions in the FMCSR, covering the HOS for drivers who operate interstate. Some aspects of these have changed effective July 1, 2013. The first deals with CDL drivers who operate within 100 air-miles (115.08 statute miles), and the other deals with Non-CDL drivers who operate within 150 air-miles (172.6 statute miles). For additional information, you can review this information on-line at the FMCSA website at:

<http://www.fmcsa.dot.gov>

If you have additional questions, call the SHA Motor Carrier Division or the MSP/CVED at the telephone numbers in Appendix C.

Refer to the FMCSR, Part 395.1.

Electronic Logging Device (ELD)

An ELD is technology that automatically records a driver's driving time and other aspects of the hours of service (HOS) allowing easier, more accurate HOS recordkeeping. The ELD rule applies to most motor carriers and drivers who are currently required to maintain records of duty status (RODS) per Part 395, 49 CFR 395.8(a). The rule applies to

commercial buses as well as trucks, and to Canada- and Mexico-domiciled drivers.

An electronic logging device (ELD) is not the same as an Automatic Onboard Recording Device (AOBRD). The technical specifications are different.

ELD's mandatory usage is set in three phases.

12/16/2016 - Awareness and Transition Phase – Voluntary use of ELD's

12/18/2017 - Compliance date for ELD use (AOBRD's installed and in use prior to 12/18/2017 will be grandfathered for 2 years. AOBRD's that become unusable prior to 12/16/2019 must be replaced with an ELD).

12/16/2019 – Mandatory use of ELD's

The ELD rule allows limited exceptions to the ELD mandate, including:

Drivers who operate under the short-haul exceptions may continue using timecards; they are not required to keep RODS and will not be required to use ELDs.

- Drivers who use paper RODS for not more than 8 days out of every 30-day period.
- Drivers who conduct drive-away-tow-away operations, in which the vehicle being driven is the commodity being delivered.
- Drivers of vehicles manufactured before 2000.
- Drivers of property-carrying commercial motor vehicles (CMVs) rented for 8 days or less

For more information on ELD's visit FMCSA's web-site at:

<https://www.fmcsa.dot.gov/hours-service/elds/faqs>

Drivers of Property-Carrying or Passenger-Carrying Vehicles – Intrastate

Maximum Allowed On-Duty Time	Maximum Driving Time	Other Requirements That Must Be Met
<p>16 consecutive hours on-duty, but then must take 8 consecutive hours off-duty before going on-duty again</p> <p>May not exceed 70 hours on-duty in 7 consecutive days if the motor carrier does not operate every day of the week</p> <p>May not exceed 80 hours on-duty in 8 consecutive days if the motor carrier operates every day of the week</p>	<p>12 hours. After this, driver can be on-duty, but not driving for another 4 hours for a total of 16 hours consecutive on-duty. The driver must then take 8 consecutive hours off-duty before driving.</p> <p>A driver that exceeds 12 hours on-duty must log all hours in a log book, including total hours of driving and on-duty time.</p>	<p>Driver must report back to their normal work reporting location at the end of their shift*</p> <p>Driver must stay within a 150 air-mile radius*</p> <p>If the driver is on-duty for a period more than 12 hours, the driver shall maintain a record of duty status (log book) that for the first 12 hours of time on-duty accounts for all time dedicated to driving*</p>

*If any of the conditions above are not met, the driver must maintain a record of duty status (log book) accounting for all hours worked as required by FMCSR's, Part 395. Refer to *"The Maryland TA, Title 25, §111*

F. PART 396 - INSPECTION, REPAIR, AND MAINTENANCE

Motor carriers shall systematically inspect, repair and maintain all vehicles subject to their control. Specific certification of the PM program is covered in *Chapter XIX, Section B* in the *Handbook*, and goes beyond the *FMCSR* summarized here.

In addition to vehicle equipment specified in the *FMCSR, Part 393*, other parts which may affect the safety of a vehicle, including frame/frame assemblies, suspension systems, axle and attaching parts, wheels and rims, and steering system, must be maintained in a safe and proper operating condition.

The following records are required (except for vehicles controlled for less than 30 days) to be maintained. The records shall be retained by the motor carrier where the vehicle is either housed or maintained for a period of 1 year, and for 6 months after the vehicle leaves the carrier's control. If the vehicle is Maryland registered, the following files must be retained for 2 years:

1. Vehicle identification (i.e., company number, vehicle year and make, etc.).
2. Record of inspections, repairs, and maintenance, including their nature and the date performed, and lubrication records.
3. A preventive maintenance schedule indicating the nature and date of the various inspection and maintenance operations to be performed.

Every driver must perform a post-trip inspection and complete a written vehicle inspection report at the end of each work day on each vehicle operated, covering at least the parts and accessories previously listed on pages 112-113 (items 1 through 8), plus all wheels and rims and emergency equipment.

The vehicle inspection report shall identify the vehicle and list any defect that would affect its safe operation or result in mechanical breakdown. In all cases, the driver shall complete and sign this report.

Before dispatching the vehicle, the motor carrier shall certify on the report that the defect has been corrected or that correction is unnecessary. This report shall be kept for three months.

Before driving the vehicle, the driver shall be satisfied that the vehicle is in safe operating condition and review the last inspection report. If defects were previously noted, the driver must sign the report acknowledging review and the carrier's certification that the required repairs were made.

NOTE: Motor Carriers operating only one motor vehicle are not required to complete a daily vehicle inspection report (known as a post-trip).

Vehicles inspected by enforcement officers that do not meet certain safety criteria by reason of their unsafe mechanical condition or loading which could likely cause an accident or a breakdown, may be placed Out-Of-Service (OOS) until repairs have been made.

**G. PART 397 - TRANSPORTATION OF HAZARDOUS MATERIALS
(Haz-Mat) - DRIVING AND PARKING RULES**

In general, this part applies to the transportation of Haz-Mat by a CMV that must be marked or placarded in accordance with the "Federal Hazardous Materials Regulations, Part 177.823."

With some limited exceptions, a CMV "which contains Division 1.1, 1.2, or 1.3 explosives shall be attended at all times by the driver or other qualified representative of the motor carrier that operates it." (FMCSR, Part 397.5)



CMV's containing "Division 1.1, 1.2, or 1.3 materials must not be parked on or within five feet of the traveled portion of a public street or highway, or on private property (including premises of a fueling or eating facility) without the knowledge and consent of the person who is in charge of the property and who is aware of the nature of the hazardous materials the vehicle contains." (FMCSR, Part 397.7)

"A motor vehicle which contains hazardous materials other than Division 1.1, 1.2, or 1.3 materials must not be parked on or within five feet of the traveled portion of public street or highway except for brief periods when the necessities of operation require the vehicle to be parked and make it impractical to park the vehicle in any other location." (FMCSR, Part 397.7)

Unless there is no practical alternative, a CMV containing Haz-Mat must not be operated:

1. Over routes that go through or near heavily populated areas.
2. Places where groups are assembled.
3. Tunnels.
4. Narrow streets, or
5. Alleys.

Operating convenience is not a consideration.

CMV's must not be operated near or parked within 300 feet of an open fire. No person may smoke or carry a lighted cigarette, cigar, or other tobacco product on or within 25 feet of a CMV containing certain specified Haz-Mat, or empty tanks that have been used to transport flammable liquids.

When re-fueling, the engine of a CMV must not be running and a person shall properly control the fueling process.

The driver of any CMV that contains Haz-Mat equipped with dual tires on any axle is required to inspect them as follows:

- At the beginning of each trip, and
- Each time the vehicle is parked

Unsafe or overheated tires shall be repaired or replaced immediately. Drivers of CMV's containing Division 1.1, 1.2, or 1.3 explosives are required to possess:

- A copy of *FMCSR, Part 397*
- Instructions in the event of an accident or delay
- Proper shipping papers
- A written route plan

COMPLIANCE, SAFETY, ACCOUNTABILITY PROGRAM (CSA)

The CSA is a Federal Motor Carrier Safety Administration (FMCSA) program designed to improve the agency's effectiveness in monitoring motor carrier and driver operations and enforcing the "Federal Motor Carrier Safety Regulations" (*FMCSR*). CSA is designed to "hold carriers and drivers accountable for their safety performance, demanding and enforcing safe on-road performance." For additional information on the CSA, visit the FMCSA website:

<http://csa.fmcsa.dot.gov>

A crucial component of CSA is the Safety Management System (SMS). The SMS is an automated system that includes motor carrier data on road safety performance. FMCSA will use the "SMS results and serious violations contained in the Behavioral Analysis and Safety Improvement Categories (BASICS) to prioritize its enforcement resources." (FMCSA website) Carriers are encouraged to check their SMS online at the FMCSA website:

<http://ai.fmcsa.dot.gov/sms>

Additionally, FMCSA's Pre-Employment Screening Program (PSP) "is designed to assist the motor carrier industry in assessing individual operators' crash and serious safety violation history as a pre-employment condition." For more information visit the FMCSA website:

<http://psp.fmcsa.dot.gov>

DATA Q'S

FMCSA has established the DATA Q process to enable carriers and owners of CMV's to challenge violations that were listed on the vehicle/driver inspection that they feel were listed in error. If you feel that a violation was listed in error, follow the process for the DATA Q challenge at FMCSA's DATA Q webpage:

<https://dataqs.fmcsa.dot.gov>

CHAPTER XXI

Carriers: Must be registered through the E-Modal System.

For more information visit their website at:

<http://www.emodal.com>

For additional information call: 1-866-758-3838

Or email them at: customersupport@emodal.com

Drivers: To enter the Port must have a current Transportation Worker Identification Credential (TWIC). For more information visit the following website at:

<http://www.tsa.gov/twic>

or

<http://homeport.uscg.mil/twic>

If you do not have a TWIC you will need to be escorted into the Port terminal. For addition information, visit:

www.marylandports.com

At the site use the drop-down menu and look for Port Security.

CHAPTER XVIII

TRAFFIC CRASH REPORTING

GENERAL REQUIREMENTS

Maryland law requires the driver of a vehicle involved in a crash to do several things. **If you are involved in a crash you are required to stop immediately, return to the scene and remain there until the following requirements have been fulfilled:**

- 1) Give reasonable assistance to any person injured.
- 2) Give your name, address, and the registration number of the vehicle you are driving.

B. NOISE ABATEMENT PROGRAM

Maryland's Noise Abatement Program (NAP) specifies a person may not operate or permit to be driven on a highway of this State any motor vehicle, or combination of vehicles at any time or at any speed, under any conditions of grade, load, acceleration, or deceleration in a manner which would exceed the sound level limits as specified in the "Code of Maryland Regulations" (COMAR).

The MSP/CVED has trained inspectors equipped with sound measurement devices to enforce this program. Testing for vehicles having a GVWR/GCWR in excess of 10,000 pounds is performed in accordance with the SAE Standard, "J986b Exterior Sound Level for Heavy Trucks and Buses."

Persons who have questions regarding the noise limits for vehicles having a GVWR, and GCWR in excess of 10,000 pounds as well as testing procedures should call the MSP/CVED telephone number listed in Appendix C.

CHAPTER XVII

PORT OF BALTIMORE

ACCESS TO THE PORT

Seagirt and Dundalk, the Port's two largest container facilities, are easily reached via the Keith Avenue exit off I-95, immediately north of the Fort McHenry Tunnel toll plaza. After exiting I-95, travel one mile east on Keith Avenue to Broening Highway, and south less than one mile to Seagirt. The Dundalk Marine Terminal is just beyond Seagirt on Broening Highway.

For entry into the Port of Baltimore:

GLOSSARY OF TERMS

GLOSSARY

Apportionable Vehicle: Any vehicle used or intended for use in Maryland and one or more *International Registration Plan (IRP)* member jurisdictions that allocates or proportionally registers a vehicle and/or vehicles that are used for the transportation of persons for hire, or designed, used or maintained primarily for the transportation of property.

Axle Weight: The weight transmitted to the road surface by one axle, or a combination of axles.

Base Jurisdiction: For purposes of single vehicle and/or fleet registration, the jurisdiction where the registrant has an established place of business, where mileage is accrued by the fleet, and where operational records of such fleet are maintained or can be made available.

Base Plate: The registration/license plate issued by the base jurisdiction and the registration plate issued for the vehicle by any member jurisdiction.

Cab Card: Terminology includes:

1. A vehicle registration card issued only by the base jurisdiction for a vehicle of an apportioned vehicle or fleet which identifies:
 - a) The vehicle, and
 - b) Base registration/license plate, and
 - c) Registered weight reflecting the jurisdictions where the vehicle is properly registered.
2. An *IFTA* license cab card.

CDL: Commercial Driver's License.

CFR: "Code of Federal Regulations."

COMAR: "Code of Maryland Regulations."

Combination: Consists of a power unit operating in combination with other vehicles. This includes trailers, semi trailers and any other vehicle.

Commercial Motor Vehicle (CMV):

For a Maryland Commercial Driver's License (CDL) a CMV means:

Violation	CSA 2010 Points	Fines/Court Cost	Out of Service
Passing an open TWIS	Yes	Yes	
Fail to use seatbelt	Yes	Yes	
Use cell phone while driving	Yes	Yes	
Lane restriction violation	Yes	Yes	
Fail to move over or slow down for emergency/maintenance vehicles on the shoulder	Yes	Yes	
Possessing alcohol/CDS *See Note	Yes	Yes	24 hours
No medical card	Yes	Yes	1st Offense Operators of passenger carrying vehicles. 2nd or subsequent Offense for operators of property carrying vehicles.
No log (ELD/AOBRD)	Yes	Yes	Yes ** see note
Driving interstate under 21yrs	Yes	Yes	Yes
Not having at least 1 working Low Beam Headlamp when required	Yes	Yes	Vehicle Out of Service *** See note
Driving W/O medical waiver/ Skill Performance Certificate in possession	Yes	Yes	Yes

*Includes all marijuana whether obtained medically through a prescription or illegally

** Enforceable effective 04/01/2018

*** Including times when windshield wipers are required

APPENDIX - A

- A motor vehicle or combination of motor vehicles used to transport passengers or property that:

1. Has a gross combination weight rating (GCWR) or gross combination weight (GCW) of **26,001 or more pounds** and towing a trailer, or semi-trailer that has a gross vehicle weight rating (GVWR) or Gross Vehicle Weight (GVW) of over 10,000 pounds, or
2. Has a GVWR of 26,001 or more pounds, or
3. Is designed to transport **16 or more passengers, including the driver**, or
4. Is of any size and used in the transportation of materials found to be hazardous for the purposes of the "Hazardous Materials Transportation Act," and which requires the motor vehicle to be placarded under the "Hazardous Materials Regulations."

For all other regulations, a CMV means:

- A motor vehicle or combination of motor vehicles used to transport passengers or property that:

1. Has a GVWR or GCWR, or gross vehicle weight (GVW) or gross combination weight (GCW) of 10,001 pounds or more, whichever is greater; or
2. Is designed or used to transport more than **8 passengers (including the driver), for compensation; or**
3. Is designed or used to transport more than **15 passengers (including the driver), and is not used to transport passengers for compensation; or**
3. Is used to transport hazardous materials in a quantity that requires placarding

Credentials: Terminology can include/mean, but is not limited to:

- The apportioned registration card and license plate issued for vehicles registered under the *International Registration Plan (IRP)*
- Registration card and license plate for non-*IRP* registrants
- *IRP* Temporary Trip Permit
- A CDL, or non-CDL, or temporary learner's permit

- *International Fuel Tax Agreement (IFTA)* decal along with corresponding cab card, or copy of an IFTA card
- Temporary *IFTA* Trip Permit
- Oversize/Overweight Hauling Permit
- Public Service Commission operating authority
- USDOT Number issued by FMCSA
- Maryland intrastate USDOT Number
- Motor Carrier (MC) Number issued by FMCSA
- Hazardous Waste Permit
- Alcoholic Beverage Transportation Permit

CSA: Compliance, Safety and Accountability Program under the FMCSA.

CVISN: Commercial Vehicle Information Systems and Networks.

CVSA: Commercial Vehicle Safety Alliance.

Decal: Terminology can include/mean, but is not limited to:

- An *IFTA* numbered sticker which is applied to both sides of a qualifying vehicle. See *IFTA* reference in this section.
- A Commercial Vehicle Safety Alliance (CVSA) safety sticker applied to a power unit after that unit has successfully passed a Level I North American Standard Inspection.

Double-Bottom Combination: (Doubles) A vehicle combination consisting of a power unit pulling:

- Two (2) semi-trailers each 28' in length, or
- A semi-trailer and a full trailer each 28' in length.

ES: Electronic Screening, or E-Screening. A component of CVISN.

Established Place of Business: A physical structure (building) located within the base jurisdiction that is designated by a street number (address) and is owned, leased or rented by a fleet registrant. The place of business must be open for business and shall be staffed during regular business hours by one or more persons employed by the Applicant or Registrant on a permanent basis for the purpose of the general

management of the Applicant's or Registrant's trucking-related business.

FHWA: Federal Highway Administration, part of the U.S. Department of Transportation.

Fleet: A group of vehicles owned or operated as a unit.

FMCSA: Federal Motor Carrier Safety Administration, part of the U.S. Department of Transportation.

FMCSR: "Federal Motor Carrier Safety Regulations."

Freight Trailer or Semi-trailer: See Trailers.

Full Trailer: See Trailers.

Gross Combination Weight (GCW): The weight of the power unit, and any other vehicle operated in combination with the power unit including any load carried by the vehicles.

Gross Combination Weight Rating (GCWR): The value specified by the manufacturer as the maximum loaded weight capacity of a combination (articulated) motor vehicle. In the absence of a value specified by the manufacturer, the GCWR will be determined by adding the GVWR of the power unit, and the total weight of the towed unit, including any load thereon.

Gross Vehicle Weight (GVW): The weight of the vehicle plus the weight of any load thereon.

Gross Vehicle Weight Rating (GVWR): The value specified by the manufacturer as the maximum loaded weight capacity of a single vehicle. The manufacturer usually places this information on a label inside the driver's door (check around the door frame after opening the door).

ITS/CVO: Intelligent Transportation Systems/Commercial Vehicle Operations.

International Fuel Tax Agreement (IFTA): An agreement between Maryland, other states, and specific Canadian provinces for prorating fuel tax fees based on the percentage of fuel used in correlation to miles traveled by a vehicle in each jurisdiction. The District of Columbia does not participate in IFTA.

International Registration Plan (IRP): An agreement between Maryland, other states, the District of Columbia, and Canadian provinces for prorating fees between jurisdictions based on percentage of miles traveled by a fleet in each jurisdiction.

Interstate Operations: Pick-up or delivery of cargo originating or destined outside the State of Maryland (pier freight, air freight, etc.). Some warehouse freight can be interstate cargo.

Intrastate Operation: Pick-up or delivery of cargo originating and destined within the State of Maryland.

Jurisdiction: The territorial range over which an authority extends. The term includes but is not limited to a country, federal district of a country, state, county, province, territory, or possession.

Maxi-cube: A non-articulating straight truck operated in combination with:

- A semi-trailer which does not exceed 34 feet and is designed to be loaded through the semi-trailer, or
- A trailer which does not exceed 28 feet and is designed so that the truck cargo is loaded through the trailer

MDE: Maryland Department of the Environment.

MDOT: Maryland Department of Transportation.

MDTA: Maryland Transportation Authority, a modal administration under the Maryland Department of Transportation.

MDTAP: Maryland Transportation Authority Police.

MDTAP/CVSU: Maryland Transportation Authority Police, Commercial Vehicle Safety Unit.

MPA: Maryland Port Administration, a modal administration under the Maryland Department of Transportation.

MSP: Maryland State Police.

MSP/ASED: Maryland State Police, Automotive Safety Enforcement Division.

MSP/CVED: Maryland State Police, Commercial Vehicle Enforcement Division.

MVA: The Motor Vehicle Administration, a modal administration under the Maryland Department of Transportation.

Member Jurisdiction: A state, or states, the District of Columbia* and Provinces of Canada which are members of *IRP* and *IFTA*. ***The District of Columbia does not participate in *IFTA*.**

Motor Carrier: An individual, partnership, corporation, or other public or private entity engaged in the transportation of goods or persons.

Motor Carrier Number or MC Number: Number issued to interstate for-hire carriers by the FMCSA.

Non-freight Trailer or Semi-trailer: See Trailers.

Operational Records: Documents supporting miles traveled in each jurisdiction and total miles traveled such as fuel reports, trip sheets and logs.

OOS: Out-Of-Service.

Permit: In common usage, the written authority issued by a jurisdiction allowing specified operation. In *IRP* states, a Trip Permit is a temporary permit issued by a jurisdiction to a motor carrier registered in another jurisdiction in lieu of regular apportioned registration. Other temporary permits issued for varying lengths of time include Oversize/Overweight Hauling Permits, Temporary *IFTA* Permits, Hazardous Waste Permits and others which are not listed here.

PSC: Maryland Public Service Commission.

Registrant: A person in whose name a vehicle is properly registered.

Registration Year: The twelve-month period during which the registration plates issued by the base jurisdiction are valid according to the laws of that jurisdiction.

Restricted Plate: A plate that has time restriction (3 months, 6 months, partial year), geographic area, mileage or commodity restrictions (farm, log or dealer plates).

SAE: Society of Automotive Engineers.

Semi-trailer: See Trailers.

SHA: State Highway Administration, a modal administration under the Maryland Department of Transportation.

SHA/HPU: State Highway Administration, Hauling Permits Unit.

SHA/MCD: State Highway Administration, Motor Carrier Division.

Stinger-Steer Automobile Transporter: A truck tractor-semi-trailer combination designed for and used to transport automobiles or boats. The fifth wheel is on a drop frame behind and below the rear axle of the power unit.

TA: *Maryland Transportation Article*. Also known as the *Maryland Vehicle Law*.

Trailers:

Freight Trailer - A freight trailer or semi-trailer shall be:

- (1) Designed for towing by a Class "E" truck
Class "F" truck tractor; and
- (2) Is in excess of 20,000 pounds' gross weight if towed by a Class "E" truck; or
- (3) Is in excess of 10,000 pounds' gross weight if towed by a Class "F" truck tractor.

Full Trailer - A vehicle without motive power, designed for:

- (1) Carrying persons or property; and
- (2) For being drawn by a motor vehicle; and

- (3) Is so constructed that no part of its weight or load rests upon or is carried by the towing vehicle.

Non-Freight Trailer - A vehicle designed for:

- (1) Towing by a Class "A" passenger vehicle, or Class "M" multipurpose vehicle, and

- Has a gross weight of 10,000 pounds or less, and is a:
 - a. Boat trailer; or
 - b. Camping trailer; or
 - c. Travel trailer; or
 - d. House trailer; or
 - e. Utility trailer.

- (2) Towing by a Class "E" truck, and

- Has a gross weight of 20,000 pounds or less

Semi-trailer - A vehicle without motive power designed for:

- (1) Carrying persons or property; and
- (2) For being drawn by a motor vehicle; and
- (3) So constructed that part of its weight and that of its load rests upon or is carried by the towing vehicle.

Tandem Axles – A group of 2 or more axles with a minimum distance from the center of the hubs of 48 inches and a maximum distance of 96 inches

Trip Permit: See Permit.

TWIC: Transportation Worker Identification Credential.

TWIS: Truck Weigh and Inspection Stations operated by either the MSP/CVED, or MDTAP/CVSU.

UCRA: "Unified Carrier Registration Act." For more information visit the following website: <https://www.ucr.in.gov/>

URS: Unified Registration System. FMCSA Registration Program that provides USDOT Numbers and MC Numbers to motor carriers and other companies.

USDOT Number: United States Department of Transportation (USDOT) Number, issued by FMCSA.

APPENDIX - B

EMERGENCIES

TELEPHONE NUMBERS FOR AGENCIES

1. Local Police, Fire Department – 24 hours
911

To report emergency traffic situations (i.e., accidents, disabled vehicles, drunk drivers, road debris, etc.) From a cellular telephone: dial **#77**

2. Maryland Department of the Environment (MDE)

CONTACT LIST

URS: Unified Registration System. FMCSA Registration Program that provides USDOT Numbers and MC Numbers to motor carriers and other companies.

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2. Maryland Department of the Environment (MDE)

Hazardous Material Spill and Oil Spill
Response (24 hour)
1-866-633-4686

Division of Radiation Control
(Monday through Friday
7:30 AM - 5:00 PM)

1-410-537-3300

(Monday through Friday,
including weekends and
holidays - 4:30 PM - 8:30 AM)

1-866-633-4686

**3. Maryland State Police HQ/State Fire
Marshal** (MSP monitors CB Channel 9)

1-410-486-3101

Toll-free - Emergencies

1-800-525-5555

**4. Maryland Transportation Authority Police
(MDTAP)** - MDTAP monitors CB Channel 3

1-888-754-0185

**5. National Information Reporting & Response
Center (USDOT)**

1-800-424-8802

**6. Chemical Transportation Emergency Center
(CHEMTREC)**

1-800-424-9300

7. CSX Railroad Grade Crossing

1-800-232-0144

8. Norfolk Southern Railroad Grade Crossing

1-800-453-2530

**9. Maryland State Highway Administration
Motor Carrier Division
Hauling Permits Unit**

**Emergency Hauling Permits – Non-
Business hours
Statewide Operations Center (SOC)**

1-800-543-2515

APPENDIX - C

GENERAL CONTACT INFORMATION FOR AGENCIES

The office hours for the below listed agencies in most cases are from 8:00 AM to 5:00 PM, Monday through Friday.

1. Motor Vehicle Administration (MVA)

Headquarters

6601 Ritchie Highway

Glen Burnie, MD. 21062

Website: <http://www.mva.maryland.gov>

General Information

1-410-768-7000

Motor Carrier Services Section,

Room 120, Apportioned Registration

1-410-787-2971

**Truck and Bus Driver Licensing (CDL)
Information:
1-410-768-7000**

2. Office of Freight and Multimodalism

Maryland Department of Transportation
7201 Corporate Center Drive
P.O. Box 548
Hanover, MD. 21075
1-410-865-1094

Website:

[http://www.mdot.maryland.gov/Office%20of%20Freight%20and%20Multimodalism/Freight Planning.html](http://www.mdot.maryland.gov/Office%20of%20Freight%20and%20Multimodalism/Freight%20Planning.html)

3. Maryland State Highway Administration

Office of Traffic and Safety
Motor Carrier Division
7491 Connelley Drive
Hanover, MD. 21076
Website: <http://www.roads.maryland.gov>
Email: MdMotorCarrier@sha.state.md.us

Calls for Commercial Vehicle Regulations,
Safety Programs, ITS/CVO Programs,
Maryland Motor Carrier Handbook, and
Maryland Truckers Map:

Toll Free: **1-800-543-4564**
Outside Maryland: **1-410-582-5734**

Calls for Oversize/Overweight Hauling Permits
and size & weight issues:
1-410-582-5734

**Emergency Permits Non-Business Hours:
Call the State-wide Operations Center (SOC)
1-800-543-2515**

4. Maryland State Police (MSP)

Commercial Vehicle Enforcement Division (CVED)
6855 Deerpath Road, Suite G
Elkridge, MD. 21075**

MSP Website: <http://www.mdsp.org>

1-(410) 579-5959

Automotive Safety Enforcement Division (ASED)
6601 Ritchie Highway
Glen Burnie, MD. 21062

1-410-768-7388

5. Maryland Transportation Authority Police

Commercial Vehicle Safety Unit
15 Turnpike Drive
Perryville, MD. 21903-2219

Website: <http://www.mdtta.maryland.gov>

1-410-537-1129

1-410-537-1130 (fax)

Baltimore Harbor Tunnel (I-895) **1-410-537-1230**

Fort McHenry Tunnel (I-95) **1-410-537-1230**

John F. Kennedy Highway (I-95) (MSP) **1-410-537-1150**

Nice Memorial Bridge (US 301) **1-410-537-6800**

Lane Memorial Bridge (US 50) **1-410-537-6601**

Francis Scott Key Bridge (I-695) **1-410-537-7600**

Hatem Memorial Bridge (US 40) **1-410-537-1196**

6. Comptroller of Maryland

Motor-fuel, Alcohol and Tobacco Tax Unit
(MATT)
P.O. Box 2397
Annapolis, MD. 21404-2397
Website: <http://www.comp.state.md.us>

**For Alcohol Beverage Transportation Permits:
1-410-260-7327**

**For IFTA Decals/Registration:
1-410-260-7980**

7. Public Service Commission

Transportation Division
6 St. Paul Street
Baltimore, MD. 21230
Website: <http://www.psc.state.md.us>
1-410-767-8128

8. Maryland Department of Environment

1800 Washington Blvd. - STE 645
Baltimore, MD. 21230
Website: <http://www.mde.state.md.us>

**Hazardous Materials Transportation:
1-410-537-3975**

**Oil Control Program:
1- 410-537-3442**

**Hazardous Waste Permits and
Hazardous Materials Information:
1-410-537-3400**

**Waste Minimization & Recycling (Tires):
1-410-537-3315**

9. Maryland Port Administration
2700 Broening Highway
Dundalk Marine Terminal
Intermodal Department
Baltimore, MD. 21222
Website: <http://www.mpa.maryland.gov>
1-800-638-7519 (Toll-Free)

10. The Maryland Motor Truck Association
9256 Bendix Road Suite 203
Columbia, MD. 21045
Website: <http://www.mmtanet.com>
1-410-644-4600

11. The Maryland Motor Coach Association
1-717-236-4205
Website: <http://www.marylandmotorcoach.org>

12. Federal Motor Carrier Safety Administration
1200 New Jersey Avenue, SE
Washington, DC 20590
Website: <http://www.fmcsa.dot.gov>
To Obtain a:
• **USDOT Identification Number**
• Carrier Safety Profile
• Other Federal Safety Information
1-800-832-5660

Information for: **Operating Authority –
For-Hire Carriers**

- New Applications
- Insurance Filing Information

- Safety Ratings

1-800-832-5660

Household Goods/Driver Complaints:

1-888-DOT-SAFT (1-888-368-7238)

Insurance Information:

1-202-385-2423

13. Baltimore Division Office, FMCSA

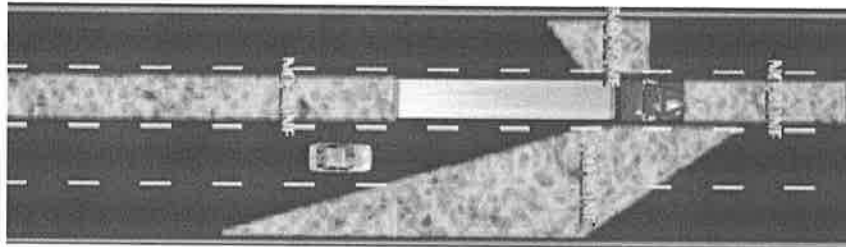
George H. Fallon Federal Building

3100 Hopkins Plaza Suite 750

Baltimore, MD. 21201

1-443-703-2360

Don't hang out in the **NO-ZONE**



The "No-Zone" represents the danger areas around trucks and buses where crashes are more likely to occur. Some No-Zones are actual blind spots or areas around trucks and buses where your car "disappears" from the view of the drivers. These blind spots are the Side No-Zone, Rear No-Zone, and Front No-Zone areas. The right-side blind spot is doubly dangerous because trucks and buses make wide right turns! Knowing the No-Zones and avoiding them can save your life!